



March 9, 2016

New York City Office of Environmental Remediation
City Voluntary Cleanup Program
% Mr. Shaminder Chawla
100 Gold Street, 2nd Floor
New York, New York 10038

Re: VCP # 15CVCP162R
Lighthouse Point
5 Bay Street, Staten Island, New York
Remedial Action Work Plan (RAWP) Stipulation List

Dear Mr. Chawla:

Roux Associates, Inc. hereby submits a Remedial Action Plan (RAWP) Stipulation List for the Site to the New York City Office of Environmental Remediation (OER) on behalf of 5 Bay Street, LLC. This letter serves as an addendum to the RAWP to stipulate additional content, requirements, and procedures that will be followed during the site remediation. The contents of this list are added to the RAWP and will supersede the content in the RAWP where there is a conflict in purpose or intent. The additional requirements/procedures include the following Stipulation List below:

1. The criterion attached in Appendix 1 will be utilized if additional petroleum containing tank or vessel is identified during the remedial action or subsequent redevelopment excavation activities. All petroleum spills will be reported to the NYSDEC hotline as required by applicable laws and regulations. This contingency plan is designed for heating oil tanks and other small or moderately sized storage vessels. If larger tanks, such as gasoline storage tanks are identified, OER will be notified before this criterion is utilized.
2. A pre-construction meeting is required prior to start of remedial excavation work at the site. A pre-construction meeting will be held at the site and will be attended by OER, the developer or developer representative, the consultant, excavation/general contractor, and if applicable, the soil broker.
3. A Historic Fill Transfer and Disposal Notification Form to each disposal facility and a pre-approval letter from all disposal facilities will be provided to OER prior to any soil/fill material removal from the site. The Historic Fill Transfer and Disposal Notification Form template is attached in Appendix 2. Documentation specified in the RAWP – Appendix 3 – Section 1.6 “Materials Disposal Off-Site” will be provided to OER. If a different disposal facility for the soil/fill material is selected, OER will be notified immediately.

4. Signage for the project will include a sturdy placard mounted in a publically accessible right of way to building and other permits signage will consist of the NYC VCP Information Sheet (attached Appendix 3) announcing the remedial action. The Information sheet will be laminated and permanently affixed to the placard.
5. If your site contains hazardous waste that will be excavated and disposed of offsite, OER can work with your development team to seek an exemption for your property from the \$130/ton state Hazardous Waste Program Fee. To qualify for an exemption, your site must be enrolled in the city Voluntary Cleanup Program; hazardous waste must result from remedial action set forth in a cleanup plan approved by OER; and OER must oversee the cleanup. It is the applicant's responsibility to notify your OER Project Manager, copying supervising Project Manager and Shaminder Chawla, before hazardous waste is shipped from your site. Unless the Department of Environmental Conservation is notified before waste is shipped from your site, you may not receive an exemption from the fee. The exemption does not cover, and you remain liable for, the Special Assessment on Hazardous Waste (established by ECL§ 27-0923) which charges a fee of up to \$27 per ton for hazardous waste generated that is due at the State Department of Taxation and Finance 30 days after the end of the quarter in which the waste was generated. Appendix 4 includes additional information about the Exemption for Hazardous Waste Program Fee.
6. Collection and analysis of 17 end-point samples from the bottom of the excavation to evaluate the performance of the remedy with respect to attainment of Track 1 SCOs. As part of the Remedial Investigation, four of the ten end-point samples were preemptively collected from the two foot interval beneath the proposed bottom of excavation. End-point samples did not exceed Track 1 SCOs, with the exception of nickel at 100 mg/kg in one sample. The presence of nickel is likely naturally occurring and not representative of contamination at the Site. The concentration is consistent with the NYSDEC background range for soils in the Eastern United States. A map indicating end-point sampling locations is attached in Appendix 5. Samples will be analyzed for contaminants of concern VOCs, SVOCs, Metals, PCBs, and Pesticides.
7. OER requires parties seeking City Brownfield Incentive Grants to carry insurance. For a cleanup grant, both the excavator and the trucking firm(s) that handle removal of soil must carry or be covered under a commercial general liability (CGL) policy that provides \$1 million per claim in coverage. OER recommends that excavators and truckers also carry contractors pollution liability (CPL) coverage, also providing \$1 million per claim in coverage. The CGL policy, and the CPL policy, if obtained, must name the City of New York, the NYC Economic Development Corporation, and Brownfield Redevelopment Solutions as additional insured. For an investigation grant, an environmental consultant must be a qualified vendor in

the BIG program and carry \$1 million of professional liability (PL) coverage. A fact sheet regarding insurance is attached as Appendix 6.

8. Daily reports will be provided during active excavation work. If no work is performed for extended time period, daily report frequency will be reduced to weekly basis. Daily report template is attached in Appendix 7.
9. Monthly reports will be provided by the owner/developer after excavation work is completed for the duration of the construction period. Monthly report template is attached in Appendix 8.
10. Trucking log sheets will be utilized as trucks are transported from sites, and completed logs should be attached to the Remedial Action Report (RAR) as an appendix. The goal of this log is to clearly document the destination of material leaving the site, the parties responsible for its transfer, and other pertinent details. The trucking log template is provided in Appendix 9.
11. A 20-mil vapor barrier will be installed beneath the structure's slab and along foundation sidewalls. The barrier chosen for this project is manufactured by Grace Bituthene. Appendix 10 provides manufactures specifications and PE/RA certified building plans with the extent of the vapor barrier installation details (penetrations, joints, etc.) with respect to the proposed foundation, footings, etc.
12. An engineered composite site cover will be placed over the entire footprint of the Site. The composite cover system will be comprised of concrete foundation/slabs. Drawings of the composite site cover are provided as Appendix 11.
13. Truck route is included in Appendix 12.
14. Updated development plans are attached in Appendix 13.
15. Updated project description in Appendix 14.

Please don't hesitate to contact me if you have any comments or questions.

Sincerely,

ROUX ASSOCIATES, INC.



Sin Senh
Principal Hydrogeologist/
Vice President

Attachments

cc: William Wong, NYCOER

APPENDIX 1

Generic Procedures for Management of Underground Storage Tanks Identified under the NYC VCP

Prior to Tank removal, the following procedures should be followed:

- Remove all fluid to its lowest draw-off point.
- Drain and flush piping into the tank.
- Vacuum out the “tank bottom” consisting of water product and sludge.
- Dig down to the top of the tank and expose the upper half.
- Remove the fill tube and disconnect the fill, gauge, product, vent lines and pumps. Cap and plug open ends of lines.
- Temporarily plug all tank openings, complete the excavation, remove the tank and place it in a secure location.
- Render the tank safe and check the tank atmosphere to ensure that petroleum vapors have been satisfactorily purged from the tank.
- Clean tank or remove to storage yard for cleaning.
- If the tank is to be moved, it must be transported by licensed waste transporter. Plug and cap all holes prior to transport leaving a 1/8 inch vent hole located at the top of the tank during transport.
- After cleaning, the tank must be made acceptable for disposal at a scrap yard, cleaning the tanks interior with a high pressure rinse and cutting the tank in several pieces.

During the tank and pipe line removal, the following field observations should be made and recorded:

- A description and photographic documentation of the tank and pipe line condition (pitting, holes, staining, leak points, evidence of repairs, etc.).
- Examination of the excavation floor and sidewalls for physical evidence of contamination (odor, staining, sheen, etc.).
- Periodic field screening (through bucket return) of the floor and sidewalls of the excavation, with a calibrated photoionization detector (PID).

Impacted Soil Excavation Methods

The excavation of the impacted soil will be performed following the removal of the existing tanks. Soil excavation will be performed in accordance with the procedures described under Section 5.5 of Draft DER-10 as follows:

- A description and photographic documentation of the excavation.
- Examination of the excavation floor and sidewalls for physical evidence of contamination (odor, staining, sheen, etc.).
- Periodic field screening (through bucket return) of the floor and sidewalls of the excavation, with calibrated photoionization detector (PID).

Final excavation depth, length, and width will be determined in the field, and will depend on the horizontal and vertical extent of contaminated soils as identified through physical examination (PID response, odor, staining, etc.). Collection of verification samples will be performed to evaluate the success of the removal action as specified in this document.

The following procedure will be used for the excavation of impacted soil (as necessary and appropriate):

- Wear appropriate health and safety equipment as outlined in the Health and Safety Plan.
- Prior to excavation, ensure that the area is clear of utility lines or other obstructions. Lay plastic sheeting on the ground next to the area to be excavated.
- Using a rubber-tired backhoe or track mounted excavator, remove overburden soils and stockpile, or dispose of, separate from the impacted soil.
- If additional UST's are discovered, the NYSDEC will be notified and the best course of action to remove the structure should be determined in the field. This may involve the continued trenching around the perimeter to minimize its disturbance.
- If physically contaminated soil is present (e.g., staining, odors, sheen, PID response, etc.) an attempt will be made to remove it, to the extent not limited by the site boundaries or the bedrock surface. If possible, physically impacted soil will be removed using the backhoe or excavator, segregated from clean soils and overburden, and staged on separated dedicated plastic sheeting or live loaded into trucks from the disposal facility. Removal of the impacted soils will continue until visibly clean material is encountered and monitoring instruments indicate that no contaminants are present.
- Excavated soils which are temporarily stockpiled on-site will be covered with tarp material while disposal options are determined. Tarp will be checked on a daily basis and replaced, repaired or adjusted as needed to provide full coverage. The sheeting will be shaped and secured in such a manner as to drain runoff and direct it toward the interior of the property.

Once the site representative and regulatory personnel are satisfied with the removal effort, verification of confirmatory samples will be collected from the excavation in accordance with DER-10.

APPENDIX 2

Historic Fill Transfer and Disposal Notification Form

Historic Fill & Soil Disposal Notification Form New York City Office of Environmental Remediation

Historic Fill & Soil Disposal Notification Form New York City Office of Environmental Remediation

Date: December 11, 2015

To operators and representatives of disposal facilities and government regulators:

The New York City Office of Environmental Remediation (OER) operates several environmental remediation regulatory programs in New York City that manage light to moderately contaminated properties that are planned for redevelopment. These projects commonly involve the removal of historical fill and soil from properties for development and other purposes. As with any environmental regulatory program, lawful transport and disposal of historic fill and soil is mandatory. It is also our highest priority.

Disposal facilities, recycling facilities and clean fill facilities (collectively, “receiving facilities”) for historic fill and soil may be located in New York or neighboring states. Our research has indicated that a wide range of facility types and a complex set of regulatory requirements and obligations for a receiving facility operation exist within each jurisdiction. Receiving facilities are required to comply with applicable laws and regulations and may operate under state and local authority via permits, licenses, registrations, agreements and other legal instruments that dictate requirements for the material they can receive. Operating requirements may include adherence to applicable chemical standards, guidance levels, criteria, policy or other bases to determine the suitability for receipt of historical fill or soil at a receiving facility. Such requirements may also specify sample frequency, location, sampling method, chemical analytes, or analytical methods. Receiving facility soil/fill sampling requirements often differ from standard remedial investigation protocol performed in the original environmental study of the property.

Given the variability of data requirements for receiving facilities, the wide range of receiving facility types, and the complexity of regulatory requirements and obligations, OER is seeking to assist government regulators and facility operators and their technical representatives to achieve compliance with regulatory requirements for disposal of historic fill and soil at receiving facilities for projects we administer. Further, we seek to ensure that all of the data and information that is developed in OER’s regulatory programs (for instance, site environmental history and soil chemistry) is available to government regulators and to facility managers when making decisions on suitability for disposal to a receiving facility.

This document provides formal notification from OER of the availability of environmental information regarding the physical and chemical content of historical fill and soil that is proposed for transfer to a disposal, recycling or clean fill facility from a property located at:

**5 Bay Street (Lighthouse Point), Staten Island, NY 10301
OER Site # 15CVCP162R**

The above referenced property has undergone regulated environmental investigation and is the subject of remedial action work plan under the authority of OER. All environmental data and information generated during this regulatory process is available online in OER’s Document Repository listed below. Be advised that many properties are also regulated under state environmental law, and additional data may be available from state agencies. OER reserves the right to share this information with applicable state regulators.

<http://www.nyc.gov/html/oer/html/document-repository/document-repository.shtml>

Note: when logged on to above URL, select the borough for the site (listed in the address above) and scroll through the list and select the address for the site (listed above). All documents are available in PDF format.

According to New York State DER-10 Technical Guidance for Site Investigation and Remediation, historical fill is non-indigenous fill material deposited on a property to raise its topographic elevation. The origin of historical fill is unknown but it is commonly known to contain ash from wood and coal combustion, slag, clinker, construction debris, dredge spoils, incinerator residue, and demolition debris. Historic fill is a regulated solid waste in the State of New York. Prior to making a determination regarding the suitability of historic fill and/or soil from this property for disposal at this receiving facility, **we strongly recommend that you review all of the data and information available for this property in our Document Repository** listed above. The repository includes:

- A Phase 1 history of use of the property;
- A Remedial Investigation Report for the property which includes:
 - Boring logs that describe physical observations of the historical fill material made by a trained environmental professional;
 - Chemical data for grab samples of historical fill collected during the remedial investigation;
- A Remedial Action Work Plan for the property.

If you have any questions, please contact Horace Zhang at (212) 788-8484 or H Zhang@dep.nyc.gov for more information.

APPENDIX 3
NYC VCP Signage



NYC Voluntary Cleanup Program

**5 Bay Street (Lighthouse Point)
Staten Island, NY 10301
Site #: 15CVCP162R**

This property is enrolled in the New York City Voluntary Cleanup Program for environmental remediation. This is a voluntary program administered by the NYC Office of Environmental Remediation.

For more information,
log on to: www.nyc.gov/oer

Or scan with smart phone:



If you have questions or would like more information,
please contact:

Shaminder Chawla at (212) 442-3007
or email us at brownfields@cityhall.nyc.gov

APPENDIX 4

Hazardous Waste Fee Exemption Fact Sheet



NYC Office of Environmental
Remediation

Exemption from the Hazardous Waste Program Fee

If your site is enrolled in the city Voluntary Cleanup Program and contains hazardous waste that will be excavated and disposed of offsite, OER can work with your development team to exempt your property from the \$130/ton state Hazardous Waste Program fee. This exemption does not cover, and you remain liable for, the Special Assessment on Hazardous Waste (established by ECL§ 27-0923).

To qualify for an exemption from the Hazardous Waste Program Fee:

1. A site must be enrolled in the city Voluntary Cleanup Program;
2. Hazardous waste must result from remedial action set forth in a cleanup plan approved by OER; and
3. OER must oversee the cleanup.

Process for obtaining a Hazardous Waste Program Fee exemption:

For each VCP site, OER will submit three certifications to the New York State Department of Environmental Conservation (DEC):

1. OER will prepare a Notice of Potential Generation after a soil test shows a site contains hazardous waste. To prepare this Notice, you must provide your OER project manager with:
 - the site's EPA generator ID number;
 - the date of the soil test confirming hazardous waste;
 - the amount of hazardous waste in tons that you anticipate shipping offsite; and
 - the anticipated dates for the start and completion of remediation.

DEC must receive this form **before** hazardous waste is shipped from your site. Otherwise your claim for an exemption may be denied.

2. After hazardous waste has been removed from the site, OER will distribute a Certification of Hazardous Waste Generation to your project team which when filled out documents how the hazardous waste was managed. Once completed, it must be signed by the generator (or site owner) and the site's Qualified Environmental Professional and returned to your OER project manager with a copy to Shana Holberston sholbertson@dep.nyc.gov and Mark McIntyre mmcintyre@cityhall.nyc.gov.

3. OER will then issue a Certification of Remedial Action that Generated Hazardous Waste to DEC representing OER's approval of how a site managed its hazardous waste.

Upon OER's submission of the last two certifications to DEC, the agency will issue a written statement exempting an individual site from the Hazardous Waste Program Fee. OER will then notify the project of the exemption.

For further information, please contact:

Shana Holberston
Program Manager
(212) 788-3220

SHolberton@dep.nyc.gov

or

Mark McIntyre
General Counsel
(212) 788-3015

MMcintyre@cityhall.nyc.gov

Contact OER to confirm that you are using the most updated version of this guidance.

Ongoing Obligations:

Regardless of the Hazardous Waste Program Fee exemption, parties must:

- File a Hazardous Waste Annual Report with DEC by March 1 of each year if your site generated 15 tons of hazardous waste or more in the relevant calendar year. For details, see <http://www.dec.ny.gov/chemical/8770.html> To set forth the basis for an exemption from the Hazardous Waste Program Fee, put an X in the Exempt Remedial box in Box H of Section 1 of the Waste Generation and Management (GM) form and in the Comments Box (at the bottom of the form) include “New York City Voluntary Cleanup Program, VCP Site Number _____); and
- Make quarterly payments of the Special Assessment on Hazardous Waste to the state Department of Taxation and Finance. For details see: <http://www.tax.ny.gov/bus/haz/hzrdwste.htm>

Contact OER to confirm that you are using the most updated version of this guidance.

APPENDIX 5

End-Point Sampling Map

V:\CAD\PROJECTS\1637\0001Y121\1637.0001Y121.01.DWG



SB-12	6/11/15
Depth (ft bls)	24 - 26
Metals	
Nickel	99

SB-9	6/15/15
Depth (ft bls)	30 - 32
Metals	
Nickel	41

SB-11	6/11/15
Depth (ft bls)	30 - 32
Metals	
Nickel	61

SB-10	6/15/15	6/15/15
Depth (ft bls)	24 - 26	24 - 26
Metals		DUP
Chromium	NE	47
Nickel	56	100
Pesticides		
4,4'-DDT	ND	3.96

- LEGEND**
- PROJECT BOUNDARY
 - PROPOSED SCOPE OF EXCAVATION BOUNDARY
 - + PROPOSED SOIL CHARACTERIZATION GRID
 - SB-11 PRE-EXCAVATION ENDPOINT SOIL SAMPLE LOCATION AND DESIGNATION
 - EP-01 PROPOSED ENDPOINT SOIL SAMPLING LOCATION AND DESIGNATION

DATA BOX KEY

SAMPLE ID	SB-9	6/15/15	← SAMPLE DATE
	Depth (ft bls)	30 - 32	← SAMPLE DEPTH
	Metals		
ANALYTES	Nickel	41	← CONCENTRATIONS

Parameter	Standards*	Standards**
VOCs (µg/kg)	NE	NE
SVOCs (µg/kg)	NE	NE
Metals (mg/kg)		
Chromium	30	180
Nickel	30	310
PCBs (µg/kg)	NE	NE
Pesticides (µg/kg)		
4,4'-DDT	3.3	7900

- mg/kg – MILLIGRAMS PER KILOGRAM
- µg/kg – MICROGRAMS PER KILOGRAM
- * – NYSDEC PART 375 UNRESTRICTED USE STANDARDS
- ** – NYSDEC PART 375 RESTRICTED RESIDENTIAL STANDARDS
- NYSDEC – NEW YORK STATE DEPARTMENT OF ENVIRONMENTAL CONSERVATION
- J – ESTIMATED VALUE
- P – THE RPD BETWEEN THE RESULTS FOR THE TWO COLUMNS EXCEEDS THE METHOD-SPECIFIED CRITERIA
- I – THE LOWER VALUE FOR THE TWO COLUMNS HAS BEEN REPORTED DUE TO OBVIOUS INTERFERENCE
- D – CONCENTRATION OF ANALYTE WAS QUANTIFIED FROM DILUTED ANALYSIS. FLAG ONLY APPLIES TO FIELD SAMPLES THAT HAVE DETECTABLE CONCENTRATIONS OF THE ANALYTE
- DUP – DUPLICATE SAMPLE
- VOCs – VOLATILE ORGANIC COMPOUNDS
- SVOCs – SEMIVOLATILE ORGANIC COMPOUNDS
- PCBs – POLYCHLORINATED BIPHENYLS
- NE – NO EXCEEDANCE
- ND – NO DETECTION
- NA – NOT ANALYZED FOR BY LABORATORY
- ft bls – FEET BELOW LAND SURFACE



Title:			
ENDPOINT SAMPLE LOCATIONS			
REMEDIAL ACTION WORK PLAN LIGHTHOUSE POINT STATEN ISLAND, NEW YORK			
Prepared For: TRIANGLE EQUITIES – 5 BAY STREET, LLC			
ROUX ROUX ASSOCIATES, INC. <i>Environmental Consulting & Management</i>	Compiled by: L.J.	Date: 09MAR16	PLATE 1
	Prepared by: J.A.D.	Scale: AS SHOWN	
	Project Mgr: L.J.	Project: 1637.0001Y003	
	File: 1637.0001Y121.01.DWG		

APPENDIX 6
BIG Program Insurance Fact Sheet



FACT SHEET – BIG PROGRAM INSURANCE REQUIREMENTS

Investigation Grants – for a developer or site owner to be eligible for a BIG investigation grant, its environmental consultant(s) must be:

- a Qualified Vendor in the BIG Program; and
- maintain Professional Liability (PL) insurance of \$1M per claim and annual aggregate.

Cleanup Grants – for a developer or site owner to be eligible for a BIG cleanup grant:

- Its general contractor or excavation/foundation contractor hired to perform remedial work must maintain Commercial General Liability (CGL) insurance of at least \$1M per occurrence and \$2M in the general aggregate. It is recommended that the general contractor or excavation/foundation contractor also maintain a Contractors Pollution Liability policy (CPL) of at least \$1M per occurrence.
- Its subcontractors who are hired by the general contractor etc. to perform remedial work at a site, including soil brokers and truckers, must also maintain a CGL policy in the amount and with the terms set forth above. It is recommended that subcontractors also maintain a CPL policy in the amount and with the terms set forth above.

The CGL policy, and the CPL policy if in force, must list the city, EDC and BRS as additional insureds, include completed operations coverage and be primary and non-contributory to any other insurance the additional insureds may have.

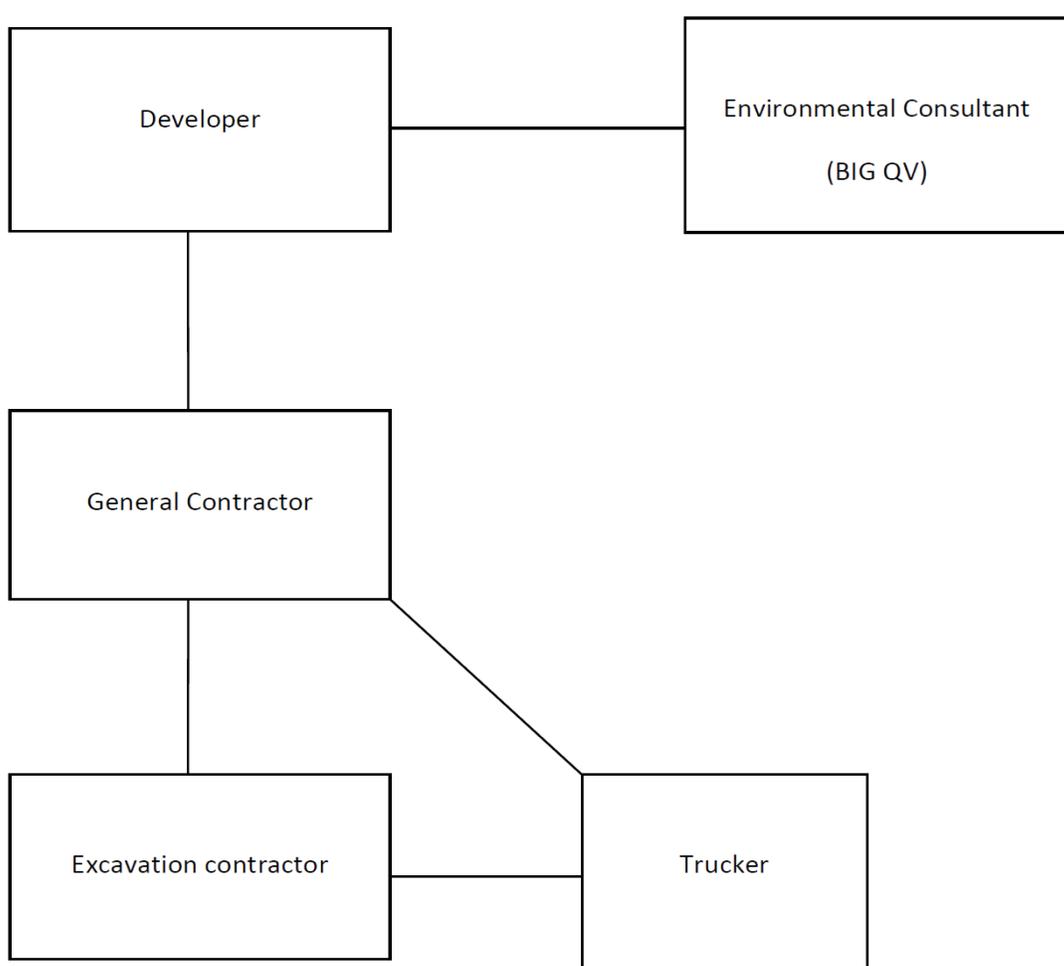
- Its environmental consultant(s) hired to oversee the cleanup must be:
 - a. a BIG Qualified Vendor; and
 - b. maintain Professional Liability (PL) insurance of \$1M per claim and annual aggregate.

If, in the alternative, the developer hires its environmental consultant to perform the cleanup, the environmental consultant must maintain CGL insurance in the amount and with the terms set forth above. It is recommended that the environmental consultant also maintain CPL coverage in the amount and with the terms set forth in the first two bulleted items listed above.

A schematic presenting the contractual relationships described above appears on page 2. Parties who must be named as Additional Insureds on Cleanup Grant insurance policies (CGL and CPL) are presented on page 3.

Example of Contractual Relationships for Cleanup Work

The Office of Environmental Remediation’s Voluntary Cleanup Plan program requires applicants to identify the parties who are engaged in active remediation of their sites including: the General Contractor hired to remediate and/or the excavation contractor hired to excavate soil from the site and the trucking firm(s) that remove soil from the site for disposal at approved facilit(ies).



The chart above shows contractual relationships that typically exist for projects that are enrolled in the Voluntary Cleanup Program.



BIG Program Additional Insureds

The full names and addresses of the additional insureds required under the Required CGL Policy and recommended CPL Policy are as follows:

“City and its officials and employees”

New York City Mayor’s Office of Environmental Remediation
253 Broadway, 14th Floor
New York, NY 10007

“NYC EDC and its officials and employees”

New York City Economic Development Corporation
110 William Street
New York, NY 10038

“BIG Grant Administrator and its officials and employees”

Brownfield Redevelopment Solutions, Inc.
739 Stokes Road, Units A & B
Medford, NJ 08055

APPENDIX 7

Daily Report Template

Generic Template for Daily Status Report

Instructions

The Daily Status Report submitted to OER should adhere to the following conventions:

- Remove this cover sheet prior to editing.
- Remove all the **red text** and replace with site-specific information.
- Submit the final version as a Word or PDF file.

Daily Status Reports

Daily status reports providing a general summary of activities for each day of *active remedial work* will be emailed to the OER Project Manager by the end of the following day. Those reports will include:

- Project number and statement of the activities and an update of progress made and locations of work performed;
- Quantities of material imported and exported from the Site;
- Status of on-Site soil/fill stockpiles;
- A summary of all citizen complaints, with relevant details (basis of complaint; actions taken; etc.);
- A summary of CAMP excursions, if any;
- Photograph of notable Site conditions and activities.

The frequency of the reporting period may be revised in consultation with OER project manager based on planned project tasks. Daily email reports are not intended to be the primary mode of communication for notification to OER of emergencies (accidents, spills), requests for changes to the RAWP or other sensitive or time critical information. However, such information will be included in the daily reports. Emergency conditions and changes to the RAWP will be communicated directly to the OER project manager by personal communication. Daily reports will be included as an Appendix in the Remedial Action Report.

DAILY STATUS REPORT

Prepared By: Enter Your Name Here

WEATHER	Snow		Rain		Overcast		Partly Cloudy	X	Bright Sun	
TEMP.	< 32		32-50		50-70	X	70-85		>85	

VCP Project No.:	16CVCP000M	E-Number Project No.:	16EHAN000M	Date:	01/01/2016
Project Name:	Name or Address				

Consultant: Person(s) Name and Company Name	Safety Officer: Person(s) Name and Company Name
--	--

General Contractor: Person(s) Name and Company Name	Site Manager/ Supervisor: Person(s) Name and Company Name
--	--

Work Activities Performed (Since Last Report):
Provide details about the work activities performed.

Working In Grid #: A1, B1, C1

Samples Collected (Since Last Report):
No samples collected or provide details

Air Monitoring (Since Last Report):
No air monitoring performed or provide details
Prestart Conditions – PID = 0.0 ppm, Dust = 0.000
High Conditions – PID = 0.0 ppm, Dust = 0.000

Problems Encountered:
No problems encountered or provide details

Planned Activities for the Next Day/ Week:
Provide details about the work activities planned for the next day/ week.

Example:

Facility # Name/ Location Type of Waste Solid <u>Or</u> Liquid	Facility # Name Location Type of Waste Solid <u>Or</u> Liquid		##### Clean Earth Carteret, NJ petroleum soils Solid							
	Trucks	Cu. Yds. <u>Or</u> Gallons	Trucks	Cu. Yds.						
Today									5	120
Total									25	600

NYC Clean Soil Bank		Receiving Facility: Name/ Address (Approved by OER)			
Tracking No.:	16CCSB000				
Today	Trucks 5	Cu. Yds. 25	Total	Trucks 120	Cu. Yds. 600

Site Grid Map
Insert the site grid map here

Photo Log

<p>Photo 1 – provide a caption</p>	<p>Insert Photo Here – Photo of the entire site</p>
<p>Photo 2 – provide a caption</p>	<p>Insert Photo Here – Photo of the work activities performed</p>
<p>Photo 3 – provide a caption</p>	<p>Insert Photo Here – Photo of the work activities performed</p>

APPENDIX 8

WEEKLY / MONTHLY STATUS REPORT

Prepared By: Enter Your Name Here

VCP Project No.:	16CVCP000M	E-Number Project No.:	16EHAN000M	Date:	01/01/2016
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Project Name:	Name or Address
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Project Updates (Since Last Report):
Provide details about the work activities performed.

Problems Encountered:
No problems encountered or provide details

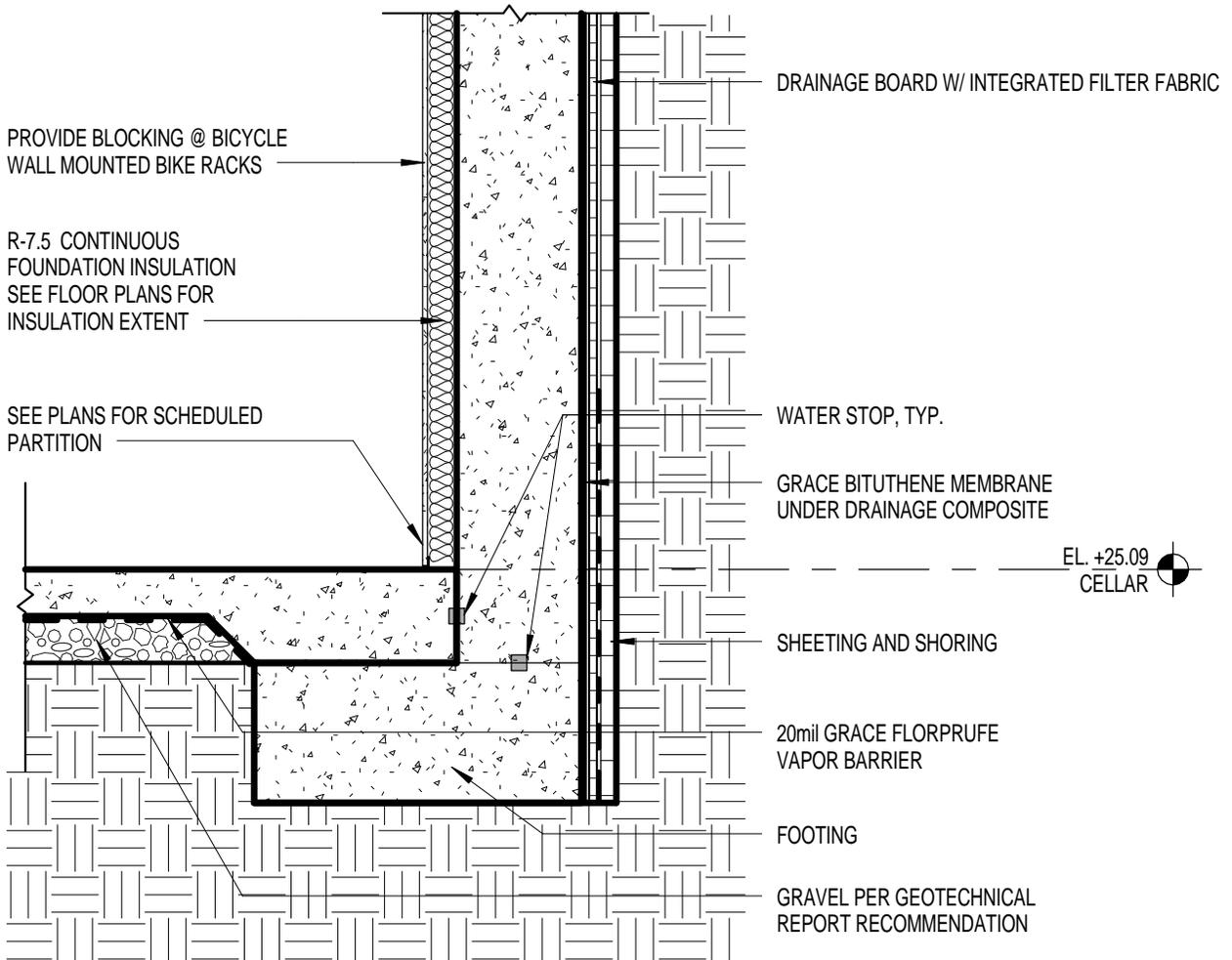
Planned Activities for the Next three months:
Provide details about the future work activities.

Photo Log

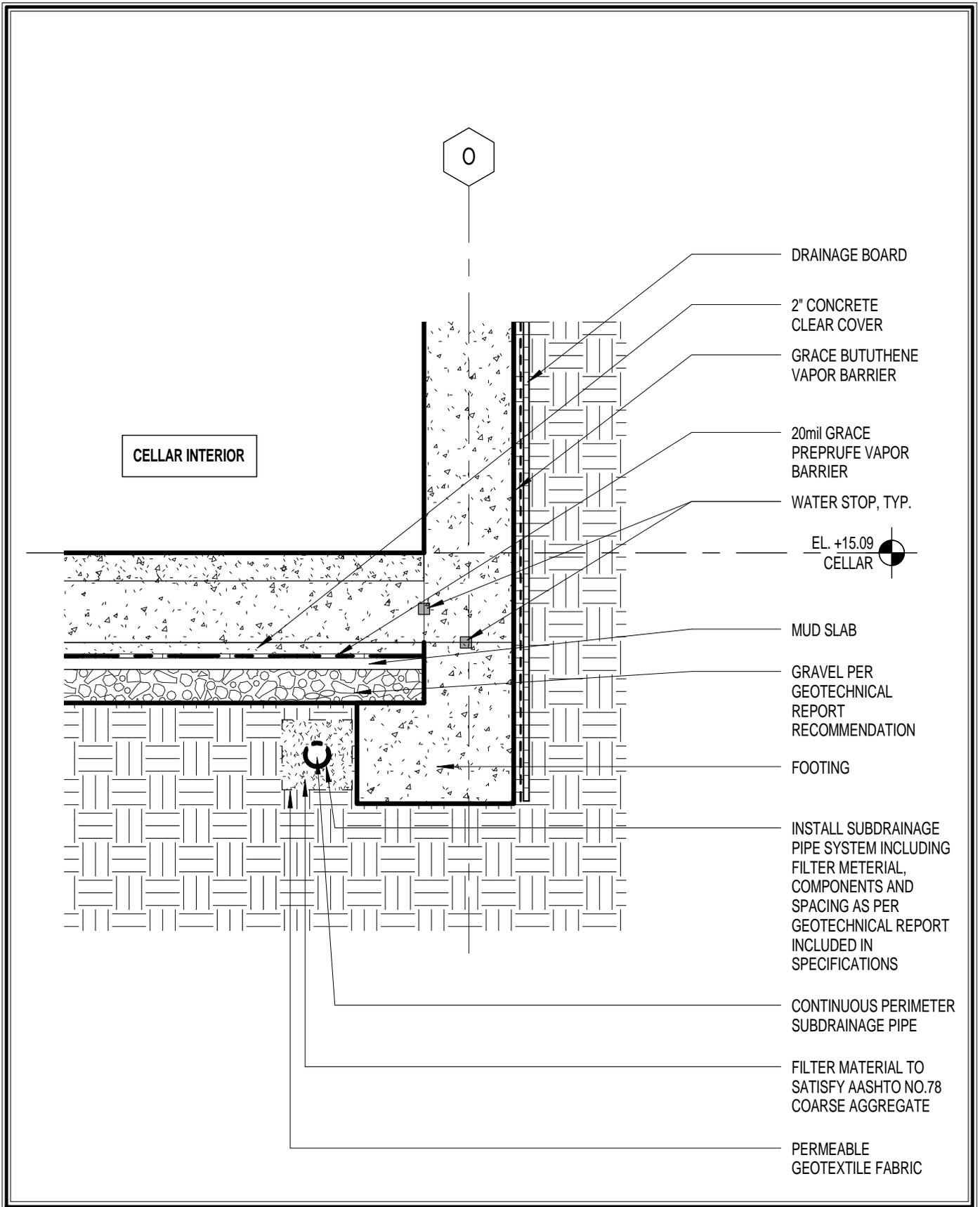
Photo 1 – provide a caption	Insert Photo Here – Photo of the entire site
Photo 2 – provide a caption	Insert Photo Here – Photo of the work activities performed
Photo 3 – provide a caption	Insert Photo Here – Photo of the work activities performed

Appendix 10

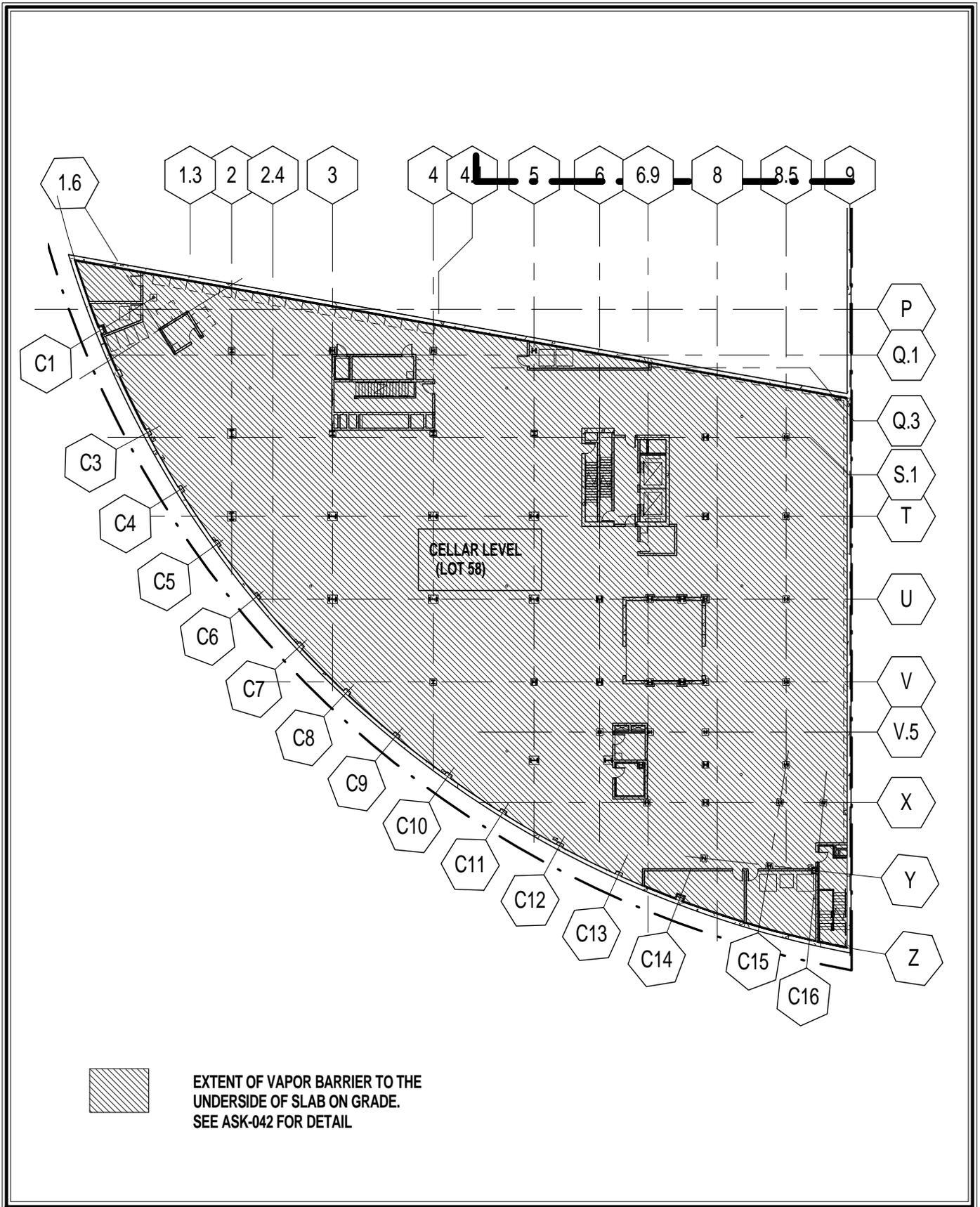
Vapor Barrier



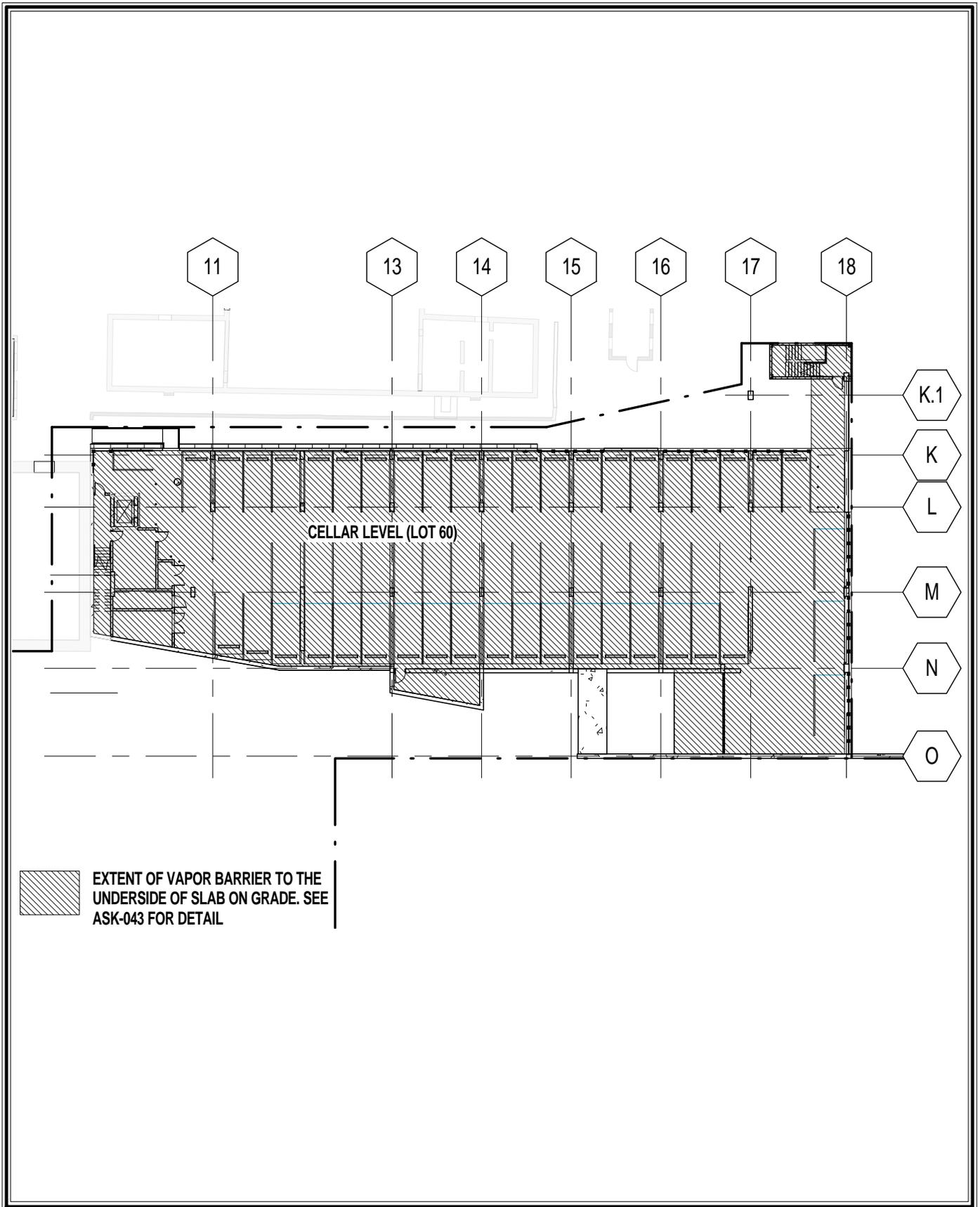
	SLAB ON GRADE DETAIL (LOT 58) <small>1/2" = 1'-0"</small>	20140263 Project No.	Drawing No. ASK-042
	LIGHTHOUSE POINT Project Name	1/A-600.00 Ref. Doc.	Date 03/07/16



 COOPER CARRY	SLAB ON GRADE DETAIL (LOT 60) <small>1/2" = 1'-0"</small>	20140263 <small>Project No.</small>	<small>Drawing No.</small> ASK-043
	LIGHTHOUSE POINT <small>Project Name</small>	4/A-502.00 <small>Ref. Doc.</small>	<small>Date</small> 03/07/16



 COOPER CARRY	SLAB ON GRADE VAPOR BARRIER_ LOT 58	20140263	Drawing No.
	LIGHTHOUSE POINT	1/A-211.00	ASK-044
	Project Name	Ref. Doc.	Date
			03/07/16



 COOPER CARRY	SLAB ON GRADE VAPOR BARRIER_LOT60 1" = 40'-0"	20140263 Project No.	Drawing No. ASK-45
	LIGHTHOUSE POINT Project Name	1/A-211.00 Ref. Doc.	Date 03/07/16

APPENDIX 11

Composite Cover System



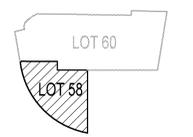
SCOPE DOCUMENTS

The Construction Documents have not been completed therefore this drawing may be incomplete or not coordinated. The documents issued indicate the general scope of the Project. The Contractor is responsible for complete and coordinated pricing and Work, and shall include all items necessary for the proper execution and completion of the Project, whether indicated or not. All components of the Project shall comply with any and all requirements of national, state, and local codes. The Contractor shall inform the Owner and Architect of any omissions, inconsistencies or errors in the information provided. If no notice is given and any omissions, inconsistencies or errors are discovered, the Architect's decisions on items of Work included in the scope shall be binding on the Contractor, when consistent with the general scope and quality of the Project.

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ISSUANCES

No.	Drawing Issue Description	Date
	Plan Review and Bid Set	07/31/15



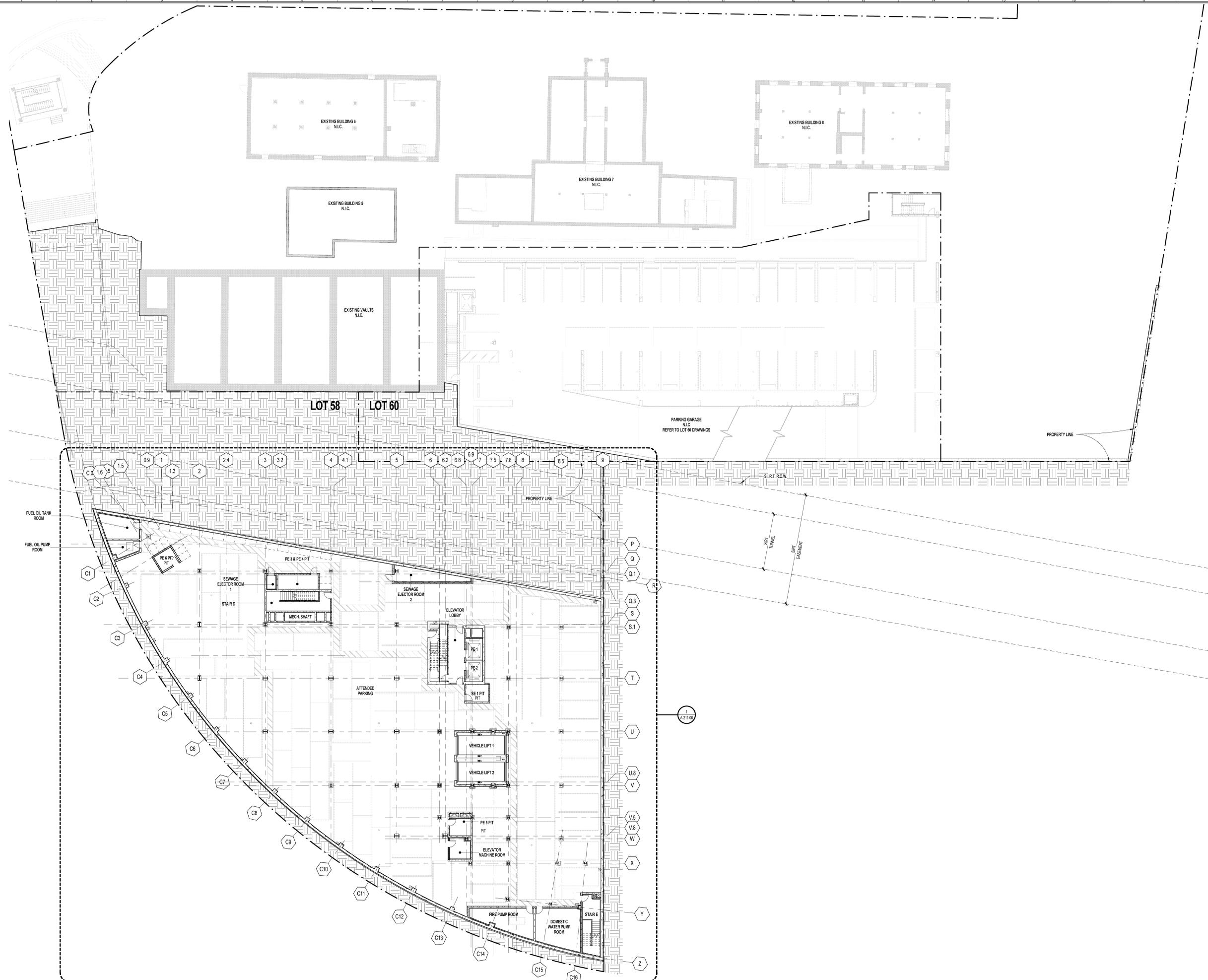
LIGHTHOUSE POINT

35 Bay Street, Staten Island, New York
Triangle Equities

FLOOR PLAN - CELLAR

JBW	20140263
Principal-in-Charge	Project No.
OP	07/31/15
Project Manager	Date
MK	
Project Architect	
JAA	
Staff Architect	

A-201.00

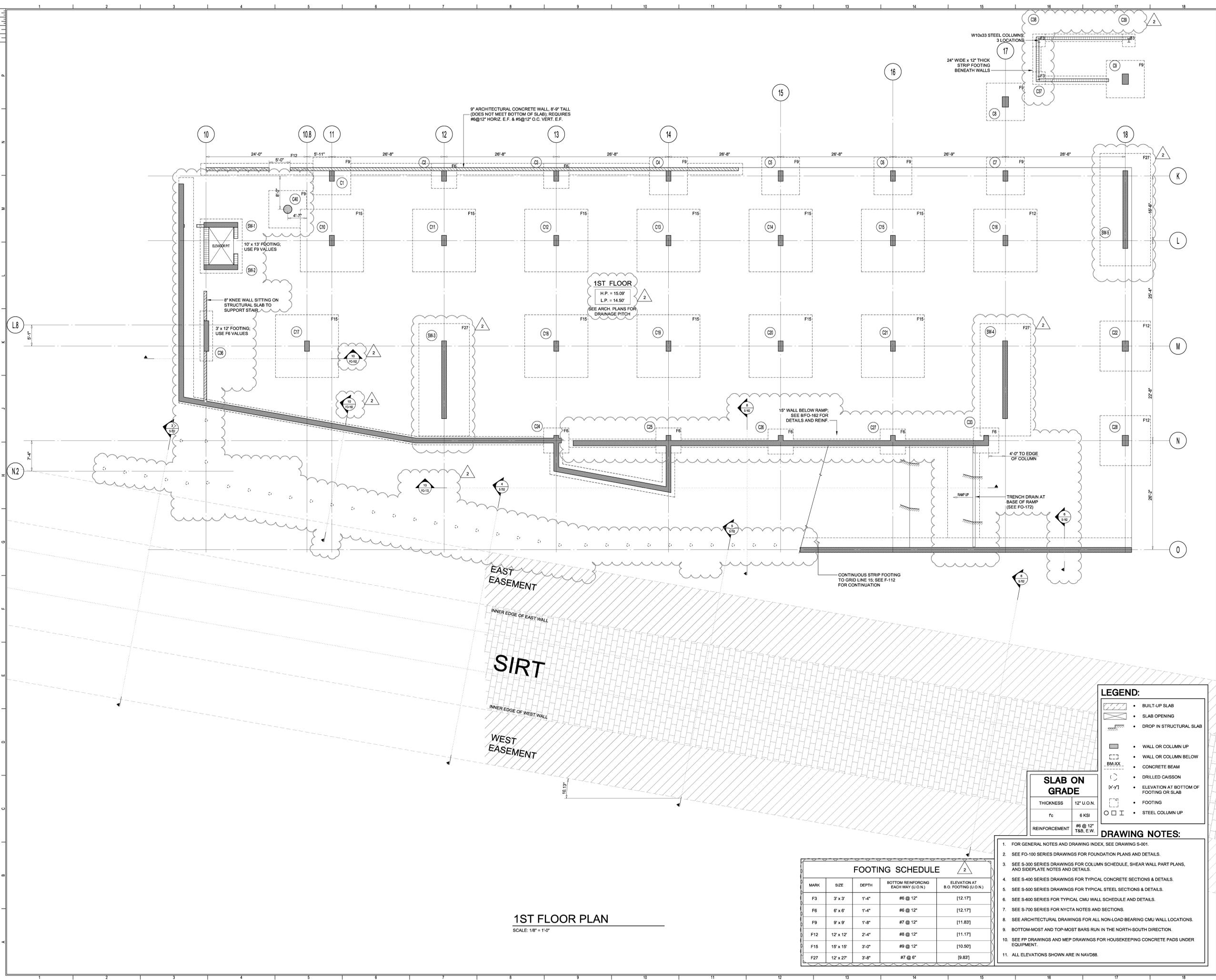


PROJECT NUMBER: 20140263 PROJECT NAME: LIGHTHOUSE POINT
SHEET NUMBER: A-201.00 - FLOOR PLAN - CELLAR
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8/2/2015 9:29:54 AM

1 CELLAR
SCALE: 1/16" = 1'-0"

SCOPE DOCUMENTS
 The Construction Documents have not been completed therefore this drawing may be incomplete or not coordinated. The documents issued indicate the general scope of the Project. The Contractor is responsible for complete and coordinated pricing and Work, and shall include all items necessary for the proper execution and completion of the Project, whether indicated or not. All components of the Project shall comply with any and all requirements of national, state, and local codes. The Contractor shall inform the Owner and Architect of any omissions, inconsistencies or errors in the information provided. If no notice is given and any omissions, inconsistencies or errors are discovered, the Architect's decisions on items of Work included in the scope shall be binding on the Contractor, when consistent with the general scope and quality of the Project.

ISSUANCES		
No.	Drawing Issue Description	Date
	PLAN REVIEW AND BID SET	07.31.15
△	ADDENDUM #2	09.14.15



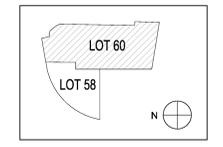
1ST FLOOR PLAN
 SCALE: 1/8" = 1'-0"

FOOTING SCHEDULE				
MARK	SIZE	DEPTH	BOTTOM REINFORCING EACH WAY (U.O.N.)	ELEVATION AT B.O. FOOTING (U.O.N.)
F3	3' x 3'	1'-4"	#6 @ 12"	[12.17]
F6	6' x 6'	1'-4"	#8 @ 12"	[12.17]
F9	9' x 9'	1'-8"	#7 @ 12"	[11.83]
F12	12' x 12'	2'-4"	#8 @ 12"	[11.17]
F15	15' x 15'	3'-0"	#9 @ 12"	[10.50]
F27	12' x 27'	3'-8"	#7 @ 6"	[9.83]

SLAB ON GRADE	
THICKNESS	12" U.O.N.
fc	6 KSI
REINFORCEMENT	#6 @ 12" T&B, E.W.

- LEGEND:**
- BUILT-UP SLAB
 - SLAB OPENING
 - DROP IN STRUCTURAL SLAB
 - WALL OR COLUMN UP
 - WALL OR COLUMN BELOW
 - CONCRETE BEAM
 - DRILLED CAISSON
 - ELEVATION AT BOTTOM OF FOOTING OR SLAB
 - FOOTING
 - STEEL COLUMN UP

- DRAWING NOTES:**
- FOR GENERAL NOTES AND DRAWING INDEX, SEE DRAWING S-001.
 - SEE FO-100 SERIES DRAWINGS FOR FOUNDATION PLANS AND DETAILS.
 - SEE S-300 SERIES DRAWINGS FOR COLUMN SCHEDULE, SHEAR WALL PART PLANS, AND SIDEPLATE NOTES AND DETAILS.
 - SEE S-400 SERIES DRAWINGS FOR TYPICAL CONCRETE SECTIONS & DETAILS.
 - SEE S-500 SERIES DRAWINGS FOR TYPICAL STEEL SECTIONS & DETAILS.
 - SEE S-600 SERIES FOR TYPICAL CMU WALL SCHEDULE AND DETAILS.
 - SEE S-700 SERIES FOR NYCTA NOTES AND SECTIONS.
 - SEE ARCHITECTURAL DRAWINGS FOR ALL NON-LOAD BEARING CMU WALL LOCATIONS.
 - BOTTOM-MOST AND TOP-MOST BARS RUN IN THE NORTH-SOUTH DIRECTION.
 - SEE FP DRAWINGS AND MEP DRAWINGS FOR HOUSEKEEPING CONCRETE PADS UNDER EQUIPMENT.
 - ALL ELEVATIONS SHOWN ARE IN NAVD88.



LIGHTHOUSE POINT

50 The Circle, Staten Island, NY
 Triangle Equities

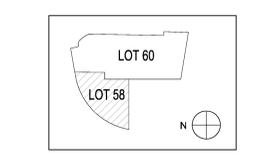
1ST FLOOR PLAN

DB	5755B
Drawn By	Project No.
BCS	09/30/14
Checked By	Date

FO-102.00

SCOPE DOCUMENTS
 The Construction Documents have not been completed therefore this drawing may be incomplete or not coordinated. The documents issued indicate the general scope of the Project. The Contractor is responsible for complete and coordinated pricing and Work, and shall include all items necessary for the proper execution and completion of the Project, whether indicated or not. All components of the Project shall comply with any and all requirements of national, state, and local codes. The Contractor shall inform the Owner and Architect of any omissions, inconsistencies or errors in the information provided. If no notice is given and any omissions, inconsistencies or errors are discovered, the Architect's decisions or items of Work included in the scope shall be binding on the Contractor, when consistent with the general scope and quality of the Project.

ISSUANCES		
No.	Drawing Issue Description	Date
	PLAN REVIEW AND BID SET	07.31.15
△	ADDENDUM #1	08.28.15



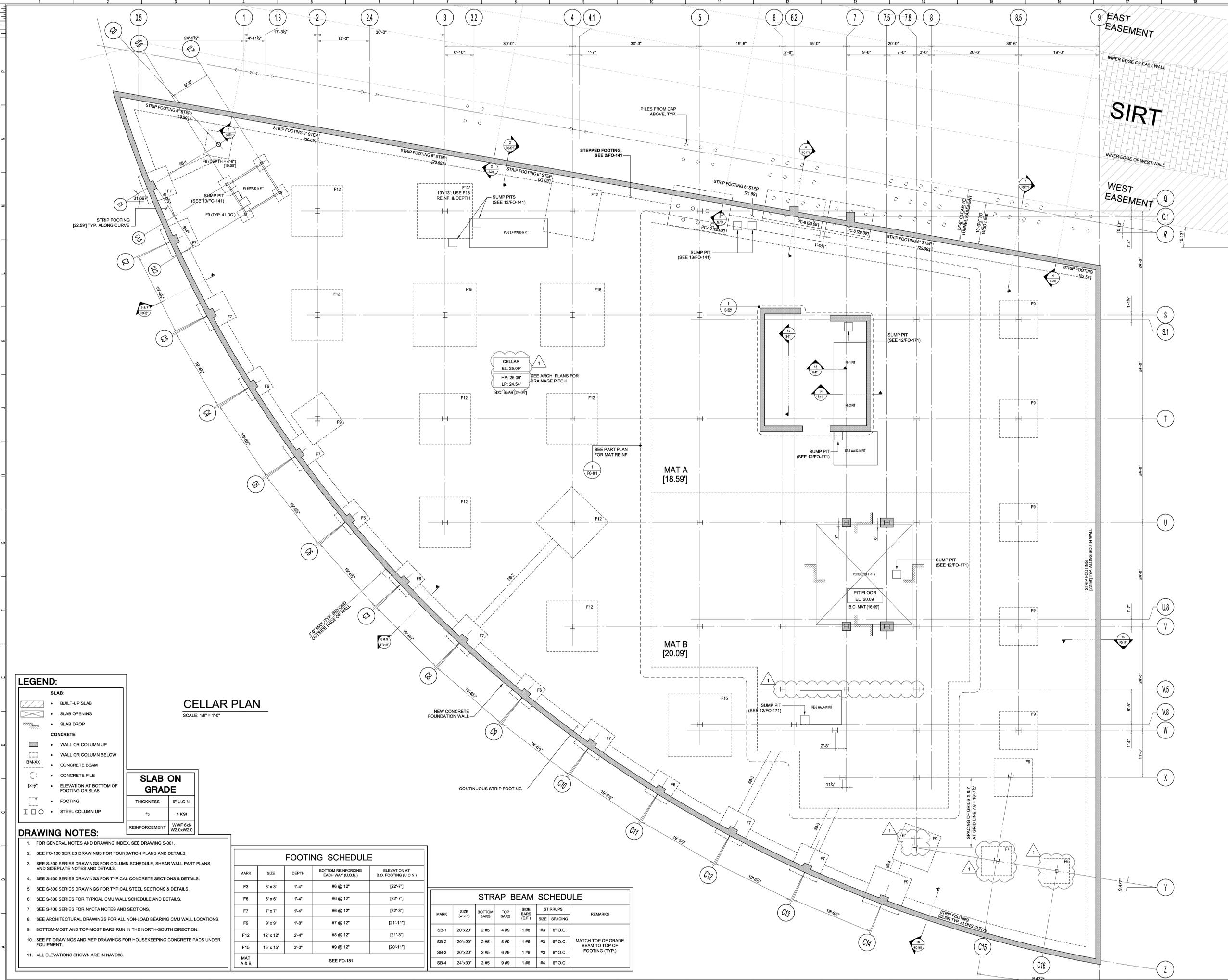
LIGHTHOUSE POINT

35 Bay Street, Staten Island, NY
 Triangle Equities

CELLAR PLAN

DB	5755B
Drawn By	Project No.
BCS	09/30/14
Checked By	Date

FO-111.00



CELLAR PLAN
 SCALE: 1/8" = 1'-0"

LEGEND:

SLAB:	
[Symbol]	BUILT-UP SLAB
[Symbol]	SLAB OPENING
[Symbol]	SLAB DROP
CONCRETE:	
[Symbol]	WALL OR COLUMN UP
[Symbol]	WALL OR COLUMN BELOW
[Symbol]	CONCRETE BEAM
[Symbol]	CONCRETE PILE
[Symbol]	ELEVATION AT BOTTOM OF FOOTING OR SLAB
[Symbol]	FOOTING
[Symbol]	STEEL COLUMN UP

SLAB ON GRADE

THICKNESS	6" U.O.N.
f _c	4 KSI
REINFORCEMENT	WWF 6x6 W2.0xW2.0

- DRAWING NOTES:**
- FOR GENERAL NOTES AND DRAWING INDEX, SEE DRAWING S-001.
 - SEE FO-100 SERIES DRAWINGS FOR FOUNDATION PLANS AND DETAILS.
 - SEE S-300 SERIES DRAWINGS FOR COLUMN SCHEDULE, SHEAR WALL PART PLANS, AND SIDEPLATE NOTES AND DETAILS.
 - SEE S-400 SERIES DRAWINGS FOR TYPICAL CONCRETE SECTIONS & DETAILS.
 - SEE S-500 SERIES DRAWINGS FOR TYPICAL STEEL SECTIONS & DETAILS.
 - SEE S-600 SERIES FOR TYPICAL CMU WALL SCHEDULE AND DETAILS.
 - SEE S-700 SERIES FOR NYCTA NOTES AND SECTIONS.
 - SEE ARCHITECTURAL DRAWINGS FOR ALL NON-LOAD BEARING CMU WALL LOCATIONS.
 - BOTTOM-MOST AND TOP-MOST BARS RUN IN THE NORTH-SOUTH DIRECTION.
 - SEE FP DRAWINGS AND MEP DRAWINGS FOR HOUSEKEEPING CONCRETE PADS UNDER EQUIPMENT.
 - ALL ELEVATIONS SHOWN ARE IN NAVD88.

FOOTING SCHEDULE

MARK	SIZE	DEPTH	BOTTOM REINFORCING EACH WAY (U.O.N.)	ELEVATION AT B.O. FOOTING (U.O.N.)
F3	3' x 3'	1'-4"	#6 @ 12"	[22'-7"]
F6	6' x 6'	1'-4"	#6 @ 12"	[22'-7"]
F7	7' x 7'	1'-4"	#6 @ 12"	[22'-3"]
F9	9' x 9'	1'-8"	#7 @ 12"	[21'-11"]
F12	12' x 12'	2'-4"	#8 @ 12"	[21'-3"]
F15	15' x 15'	3'-0"	#9 @ 12"	[20'-11"]
MAT A & B			SEE FO-181	

STRAP BEAM SCHEDULE

MARK	SIZE (w x h)	BOTTOM BARS	TOP BARS	SIDE BARS (E.F.)	STIRRUPS	REMARKS
SB-1	20"x20"	2 #5	4 #9	1 #6	#3 6" O.C.	
SB-2	20"x20"	2 #5	5 #9	1 #6	#3 6" O.C.	MATCH TOP OF GRADE BEAM TO TOP OF FOOTING (TYP.)
SB-3	20"x20"	2 #5	6 #9	1 #6	#3 6" O.C.	
SB-4	24"x30"	2 #5	9 #9	1 #6	#4 6" O.C.	



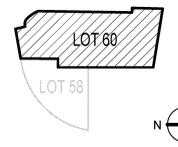
SCOPE DOCUMENTS

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ISSUANCES

No.	Drawing Issue Description	Date
1	Plan Review and Bid Set	07/31/15



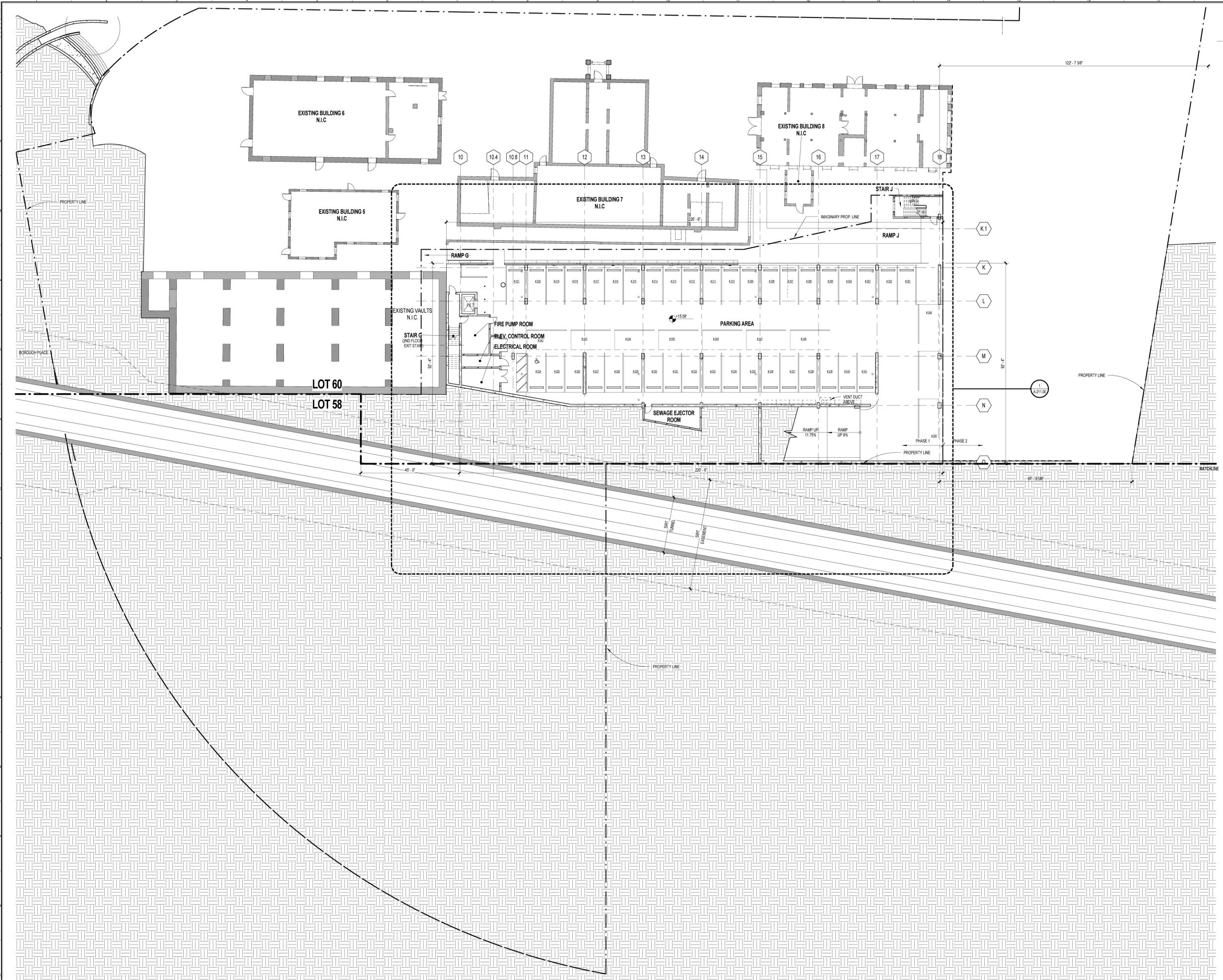
LIGHTHOUSE POINT

50 The Circle, Staten Island, New York
Triangle Equities

FLOOR PLAN - 1ST FLOOR

JBW	20140263
Principal-in-Charge	Project No.
OP	05/15/15
Project Manager	Date
MK	
Project Architect	
JAA	
Staff Architect	

A-201.00

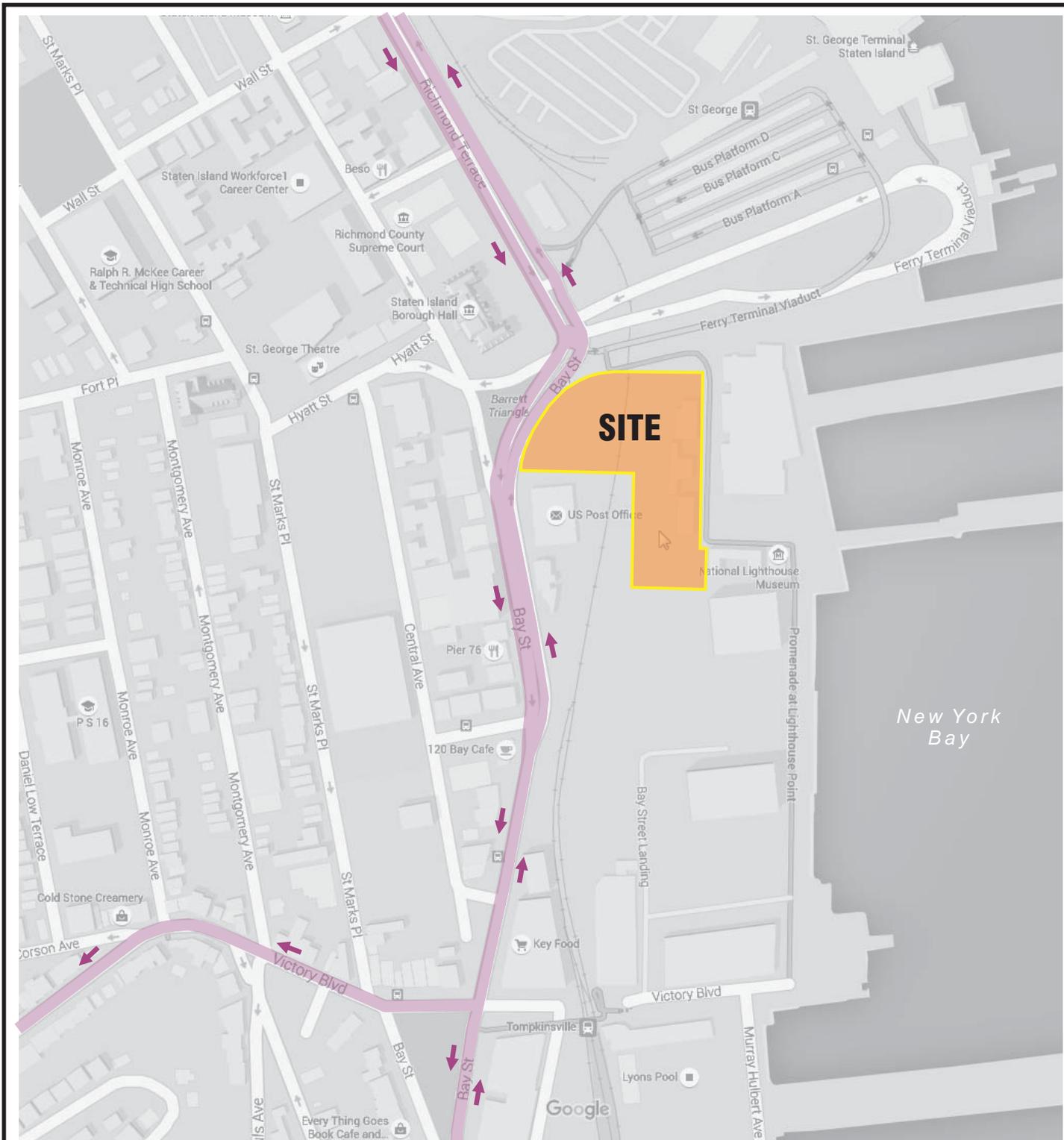


PROJECT NUMBER: 20140263 PROJECT NAME: LIGHTHOUSE POINT
SHEET NUMBER: A-201.00 - FLOOR PLAN - 1ST FLOOR
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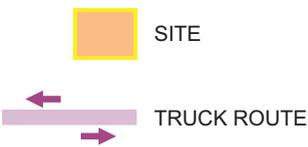
1 1ST FLOOR PLAN
A-201.00 SCALE: 1/16" = 1'-0"

APPENDIX 12

Truck Route



LEGEND



Title:

TRUCK ROUTE

REMEDIAL ACTION WORK PLAN
LIGHTHOUSE POINT
STATEN ISLAND, NEW YORK

Prepared for:

TRIANGLE EQUITIES - 5 BAY STREET, LLC

ROUX
ROUX ASSOCIATES, INC.
Environmental Consulting
& Management

Compiled by: B.K.	Date: 28OCT15
Prepared by: B.H.C.	Scale: AS SHOWN
Project Mgr.: J.C.	Project No.: 1637.0001Y003
File: 1637.0001Y117.04.CDR	

FIGURE

5

APPENDIX 13

Updated Development Plans



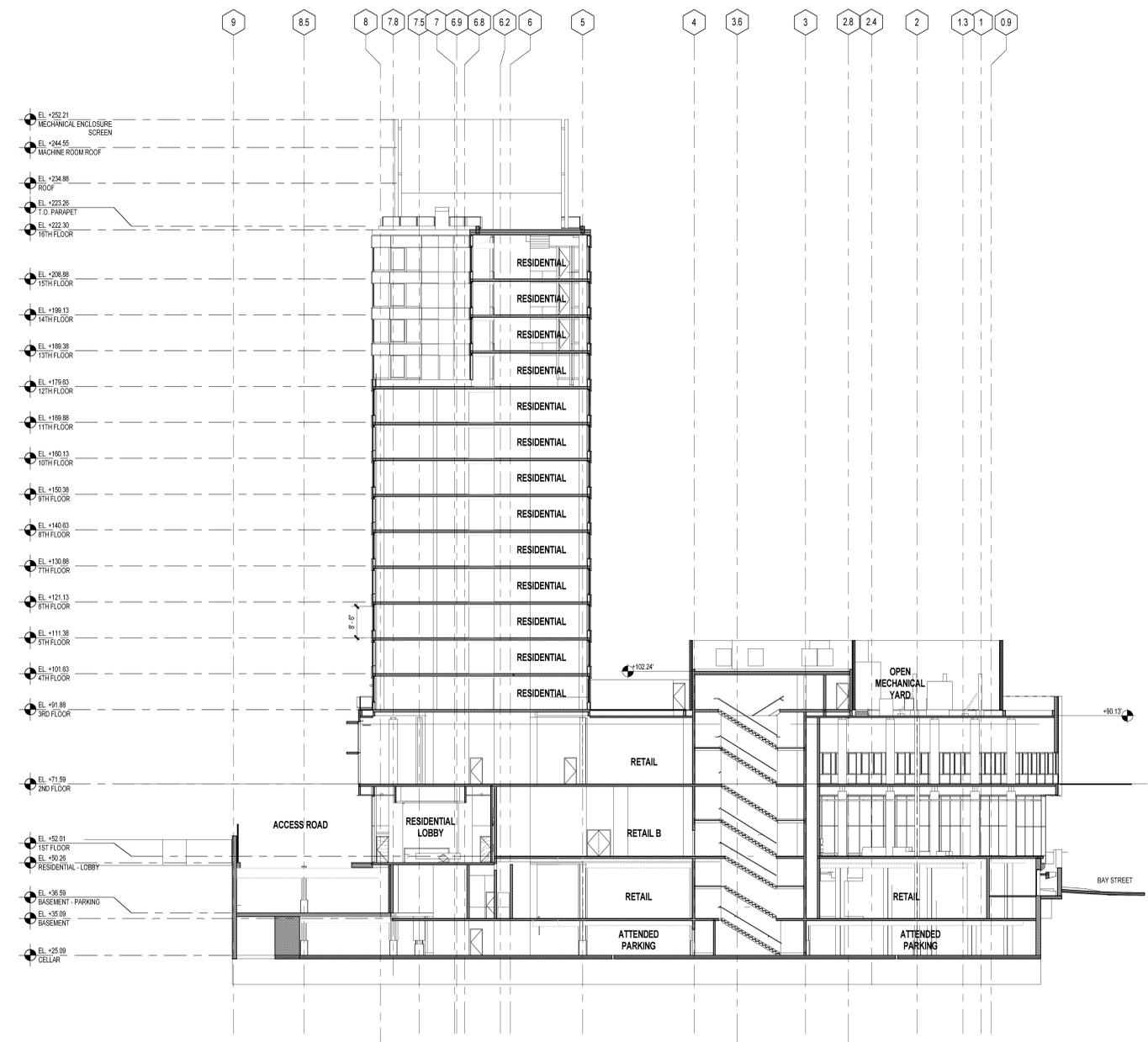
SCOPE DOCUMENTS

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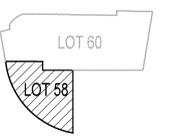
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ISSUANCES

No.	Drawing Issue Description	Date
1	Plan Review and Bid Set	07/31/15



2 BLDG - N/S SECTION 1
211.00 A-412.07 SCALE: 1/16" = 1'-0"



LIGHTHOUSE POINT

35 Bay Street, Staten Island, New York

Triangle Equities

BUILDING SECTIONS

JBW	20140263
Principal-in-Charge	Project No.
OP	07/31/15
Project Manager	Date
MK	
Project Architect	
JAA	
Staff Architect	

A-412.00

PROJECT NUMBER: 20140263 PROJECT NAME: LIGHTHOUSE POINT
SHEET NUMBER: A-412.00 - BUILDING SECTIONS
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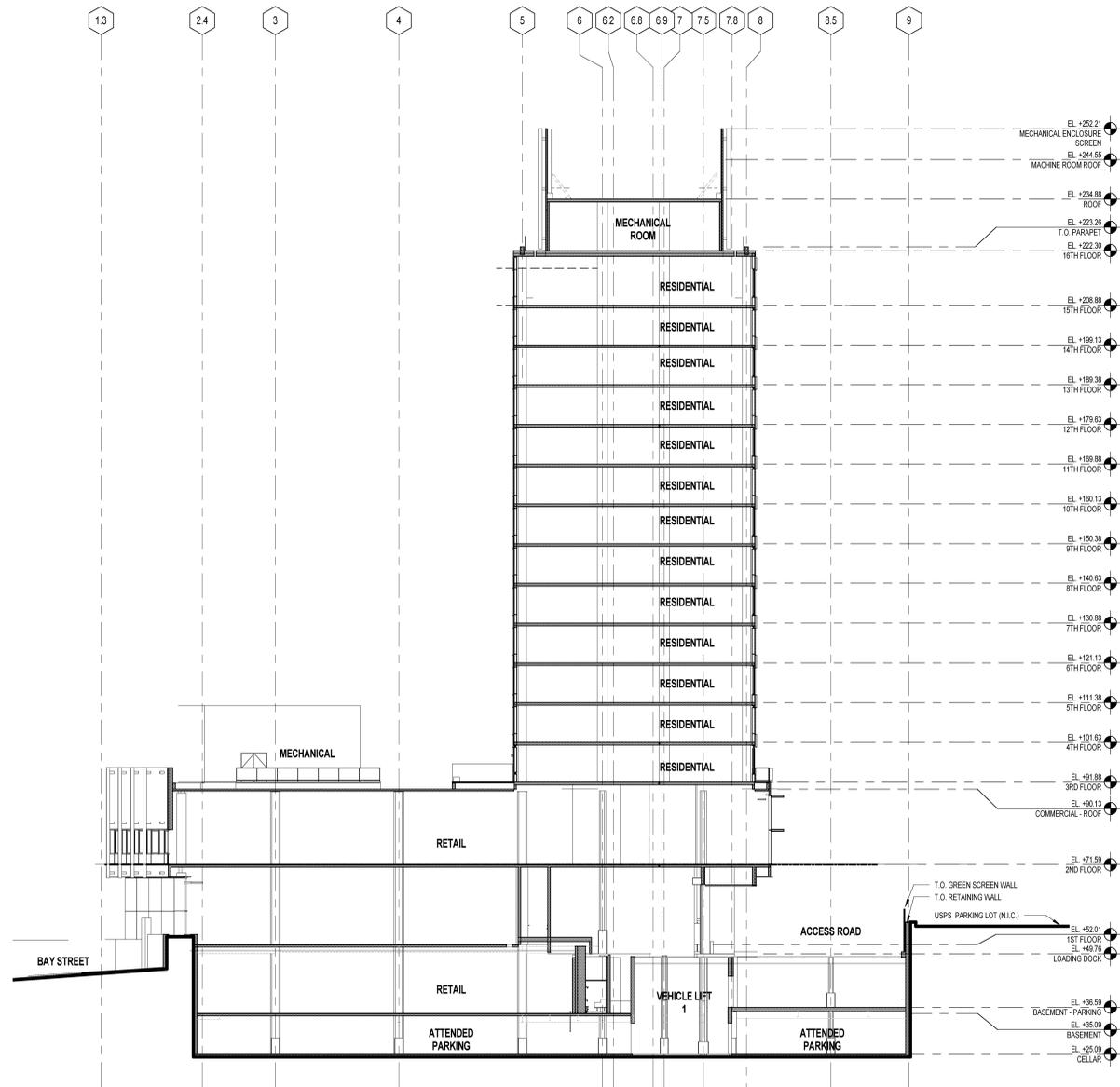
SCOPE DOCUMENTS

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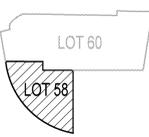
ISSUANCES

No.	Drawing Issue Description	Date
1	Plan Review and Bid Set	07/31/15



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SCALE: 1/16" = 1'-0"

PROJECT NUMBER: 20140263 PROJECT NAME: LIGHTHOUSE POINT
SHEET NUMBER: A-413.00 - BUILDING SECTIONS
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LIGHTHOUSE POINT

35 Bay Street, Staten Island, New York

Triangle Equities

BUILDING SECTIONS

JBW	20140263
Principal-in-Charge	Project No.
OP	07/31/15
Project Manager	Date
MK	
Project Architect	
JAA	
Staff Architect	

A-413.00



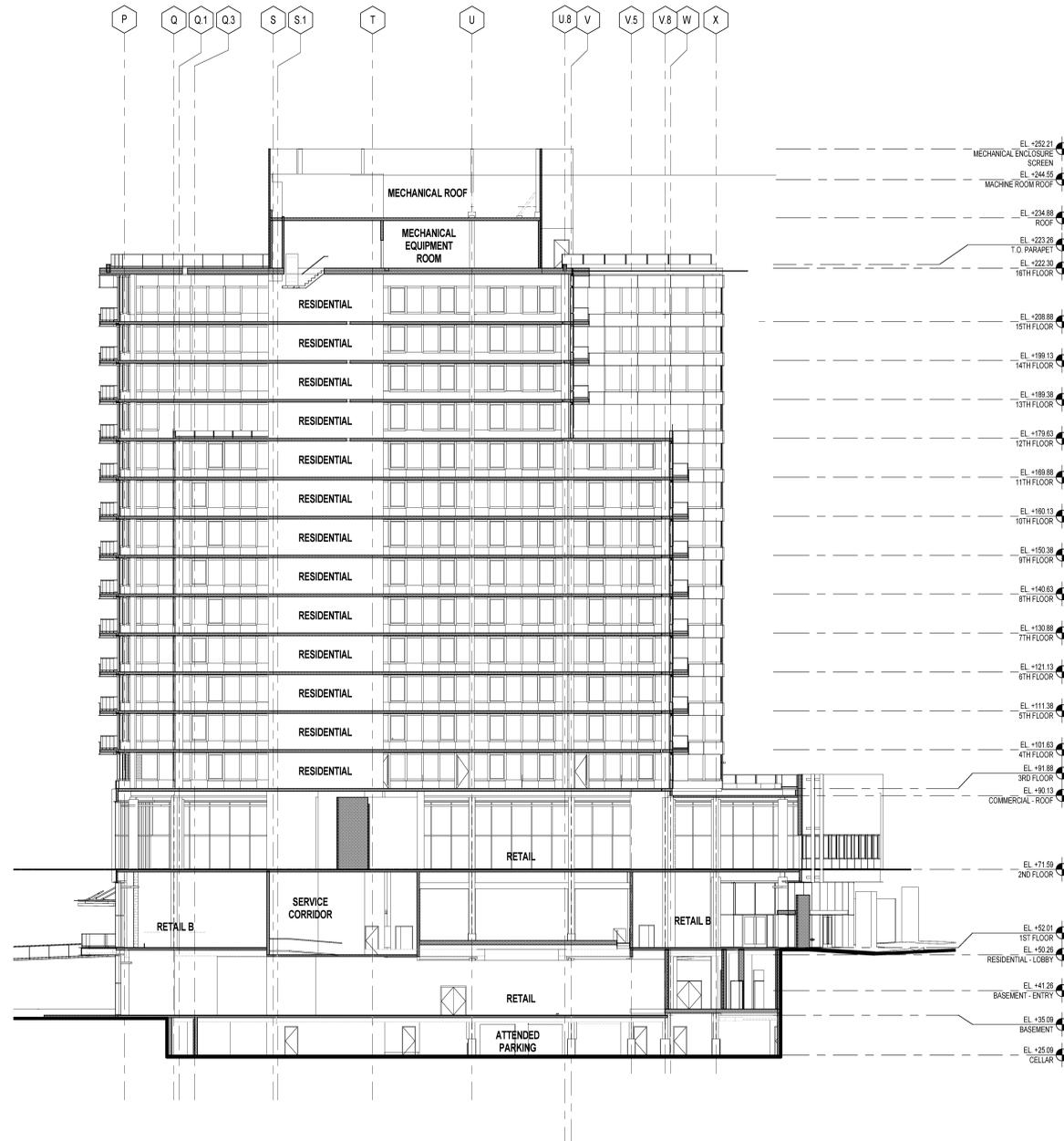
SCOPE DOCUMENTS

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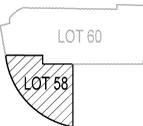
ISSUANCES

No.	Drawing Issue Description	Date
1	Plan Review and Bid Set	07/31/15



1 BLDG- E/W SECTION 1
A-411.00 SCALE: 1/16" = 1'-0"

PROJECT NUMBER: 20140263 PROJECT NAME: LIGHTHOUSE POINT
SHEET NUMBER: A-411.00 - BUILDING SECTIONS
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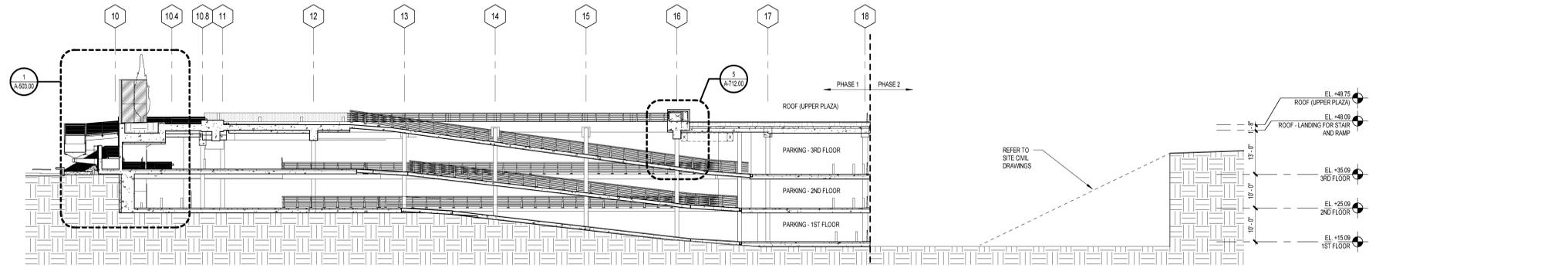
LIGHTHOUSE POINT

35 Bay Street, Staten Island, New York
Triangle Equities

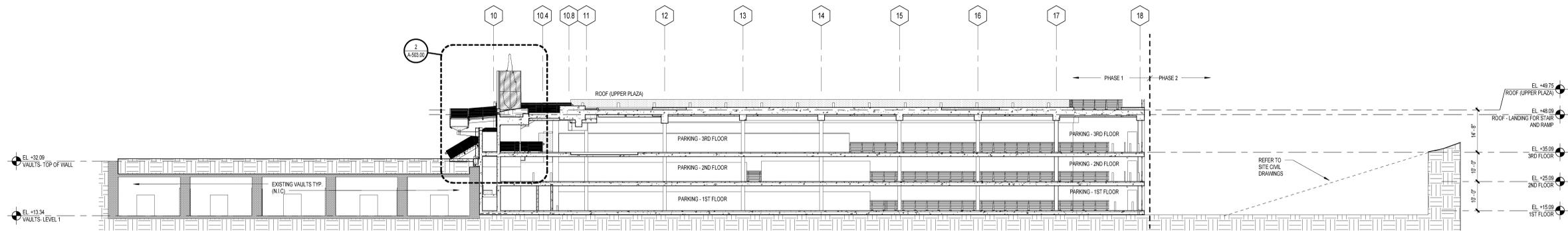
BUILDING SECTIONS

JBW	20140263
Principal-in-Charge	Project No.
OP	07/31/15
Project Manager	Date
MK	
Project Architect	
JAA	
Staff Architect	

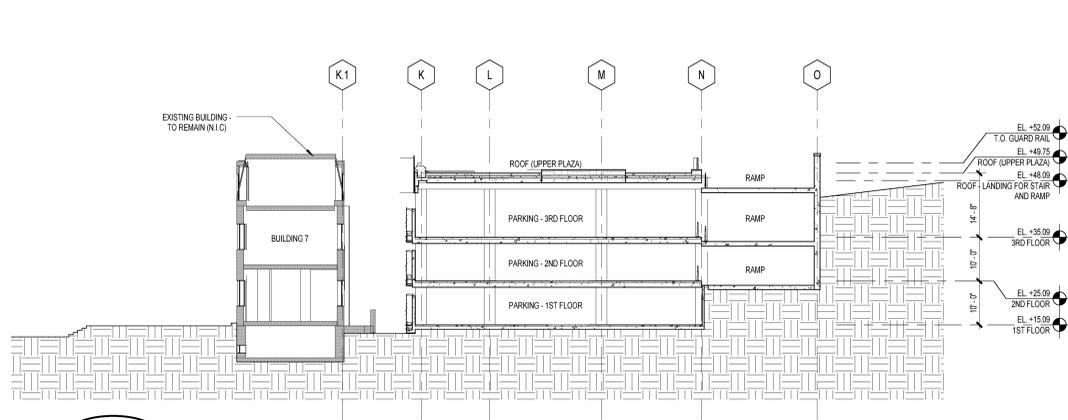
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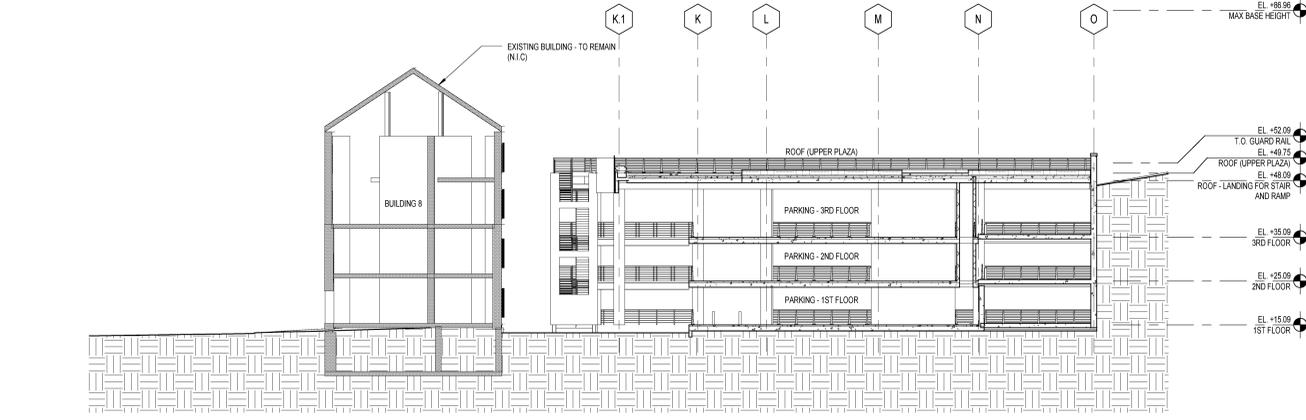
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A-211.00/A-411.00 SCALE: 1/16" = 1'-0"



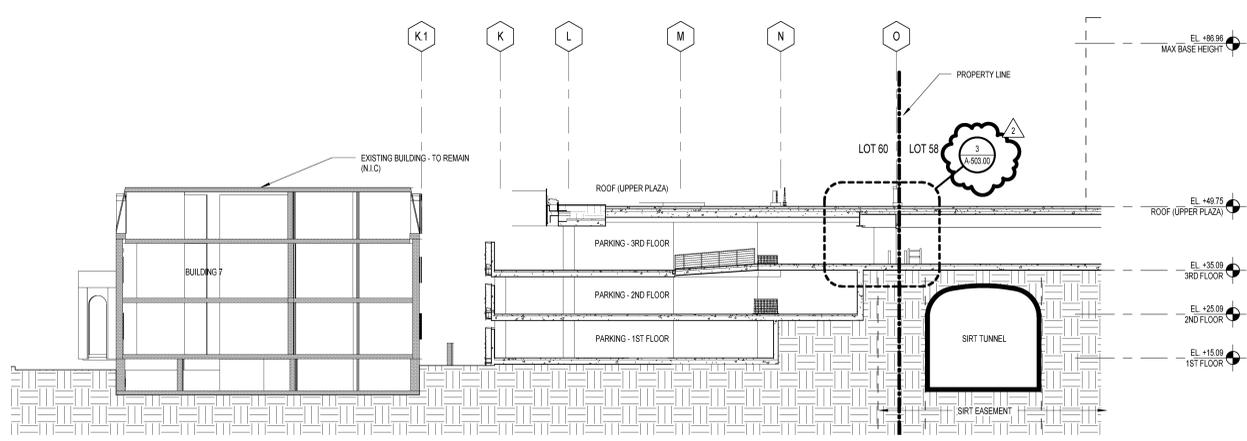
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A-211.00/A-411.00 SCALE: 1/16" = 1'-0"



3 PARKING- E/W SECTION 3
A-211.00/A-411.00 SCALE: 1/16" = 1'-0"



4 PARKING- E/W SECTION 4
A-211.00/A-411.00 SCALE: 1/16" = 1'-0"

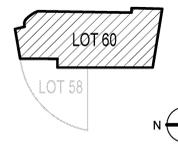


5 PARKING- E/W SECTION 5
A-211.00/A-411.00 SCALE: 1/16" = 1'-0"

SCOPE DOCUMENTS
The Construction Documents have not been completed therefore this drawing may be incomplete or not coordinated. The documents issued indicate the general scope of the Project. The Contractor is responsible for complete and coordinated pricing and Work, and shall include all items necessary for the proper execution and completion of the Project, whether indicated or not. All components of the Project shall comply with any and all requirements of national, state, and local codes. The Contractor shall inform the Owner and Architect of any omissions, inconsistencies or errors in the information provided. If no notice is given and any omissions, inconsistencies or errors are discovered, the Architect's decisions on items of Work included in the scope shall be binding on the Contractor, when consistent with the general scope and quality of the Project.
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ISSUANCES

No.	Drawing Issue Description	Date
1	Plan Review and Bid Set	07/31/15
2	Addendum #2	09/14/15



LIGHTHOUSE POINT

50 The Circle, Staten Island, New York
Triangle Equities

BUILDING SECTIONS

JBW	20140263
Principal-in-Charge	Project No.
OP	09/14/15
Project Manager	Date
MK	Project Architect
JAA	Staff Architect

A-411.00

PROJECT NUMBER: 20140263 PROJECT NAME: LIGHTHOUSE POINT
SHEET NUMBER: A-411.00 - BUILDING SECTIONS
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9/28/2015 5:29:22 PM

APPENDIX 14

Updated Project Description

The proposed future use of the Site will consist of the restoration of four historic buildings, the historic underground vaults, as well as the construction of two new buildings. Phase 1 of the development consists of the leasing of a 66,576 square foot parcel of land located at 5 Bay Street (Lot 58 and a portion of Lot 60), in Staten Island, New York; the construction and equipping of a new commercial building totaling approximately 66,295 square feet along with an approximately 96,523 square foot parking structure and 117 apartment units in a 13 story tower over the commercial space. The commercial tenants will be a mix of restaurants, supermarket and office space. Phase 2 of the development consists of the leasing of a 101,361 square foot parcel of land located at 5 Bay Street (Lot 60) in Staten Island; the restoration and equipping of four existing historic buildings totaling approximately 40,668 square feet and the construction and equipping of a new commercial building totaling approximately 108,055 along with an approximately 46,590 square foot parking structure. The proposed development will serve as a waterfront destination for tourists and local residents and will include open space areas and retail, hotel, conference center and parking facilities. Layout of the proposed Site development is presented in Figure 3. The current zoning designation is C2-4/R7A and designates commercial and residential mixed use. The proposed use is consistent with existing zoning for the property.

Lighthouse Point Alternatives Analysis for Historic Resources

I. Introduction

Proposed Action

The Lighthouse Point development is a mixed-use redevelopment located at the former United States Third District Lighthouse Depot in St. George, Staten Island (Richmond County), New York. The Lighthouse Point development would facilitate the restoration and activation of four of the deteriorated historic buildings, a set of historic underground vaults on the site, and a historic wall as well as the construction of three new buildings containing a total of approximately 96 dwelling units (DUs), 56,200 gross square feet (gsf) of local retail, a 800-seat cinema, a 146,000 gsf hotel with approximately 164 rooms, a 18,100 gsf banquet hall and approximately 345 accessory parking spaces. The proposed development would also include approximately 53,406 sf (1.2 acres) of new public open space. As shown in the attached Site Plan, the proposed development would occupy an approximately 3.34-acre portion of Lot 60 on Block 1 (Staten Island Community District 1), and is generally bound by Borough Place to the north, Bay Street to the west, the St. George Station U.S.P.S. Office to the west and south, the Bay Street Landing apartments to the south and New York City Department of Transportation (NYCDOT) Staten Island Ferry Maintenance Facility and an existing public esplanade to the east.

The construction of the proposed development is expected to be implemented in three phases. The first phase of construction would commence by year end, 2013 and would include the development of the proposed retail building along Bay Street. Phase 2 (hotel and banquet hall) and Phase 3 (mixed-use residential and retail/office building) would follow and are expected to commence approximately two years apart. It is anticipated that all construction would be completed by 2019, and that each phase of construction would have a duration of approximately 18 to 24 months.

The Applicant, 5 Bay Street LLC, is seeking approval for two discretionary actions: the disposition of City-owned property requiring both Borough Board and Mayoral approvals pursuant to New York City Charter Section 348(b)(4). The Applicant is also seeking other City approvals that are not discretionary actions including: two waterfront certifications from the Chair of the City Planning Commission (CPC) pursuant to ZR Section 61-811 for waterfront public access and visual corridors and ZR Section 61-812 for a zoning lot sub-division within a waterfront block; and approval from the New York City Industrial Development Authority (NYCIDA) for Mortgage Recording Tax exemption. Given that the project site contains (1) existing buildings and vaults that are listed on the State and National Register of Historic Places, as well as, a wall which is eligible for listing and (2) an impact on these structures is anticipated, consultation and review by the New York State Office of Parks, Recreation and Historic Preservation (NYSOPRHP) would be required under Section 14.09 of The New York State Historic Preservation Act of 1980. Additionally, one of the buildings is an individually-designated New York City Landmark and approval from the New York City Landmarks Preservation Commission (NYCLPC) for a Binding Report would also be required. Further, the Applicant is requesting from the New York City Board of Standards and Appeals (BSA) one special permit pursuant to ZR Section 73-432 for a reduction of required accessory parking spaces for places of assembly.

The purpose and goal of the Lighthouse Point development is to revitalize and enhance the historic nature of the project site and to showcase these attributes to create an immersive retail, entertainment and residential location for St. George. The development leverages the historic nature of the site as well as its waterfront location to create a unique place grounded in the specifics of the location and the neighborhood.

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Orientation

The project site is roughly divided between a Pier Level site and the upland Bay Street Level site. The two levels of the site are bisected by an underground railway and easement for the Staten Island Railroad. The project site includes four historic buildings and one historic structure at the Pier Level and a historic wall along Bay Street at the Bay Street Level.

Existing Historic Resources

A portion of the U. S. Third District Lighthouse Depot (“Depot”) is listed on the National Register of Historic Places. The National Register listing includes four historic buildings and a series of underground vaults, all of which are located on the Pier Level site. Separately, the remainder of the Depot site, which includes two additional buildings that are not part of the project, the Bay Street Level site and the surrounding perimeter wall, has been determined eligible for listing on the National Register. The four historic buildings that are part of the project are: the Administration Building, three-story brick and stone masonry building erected in 1871; the Laboratory Building, a one-story brick masonry structure erected circa 1884; the North Warehouse, a four-story brick warehouse located off the main plaza and built in 1864; and the Original Lamp Shop, a four-story brick masonry, fireproof floor warehouse built in 1868. The Underground Vaults are constructed of granite masonry built directly into the hillside of the upland portion of the site, completed in 1869. There are five vaults, which measure 21 feet wide, 51 feet long, and 13 feet high, as well as a sixth vault approximately half that size. The 10-15 foot-high historic brick perimeter wall, constructed in the mid-20th Century, surrounds the property and fronts on Bay Street (#4).

As part of the proposed redevelopment, the existing historic buildings would be restored and rehabilitated for use as part of the proposed hotel, banqueting and retail program. The restoration and rehabilitation work, described below, would comply with the Secretary of the Interior’s Standards for Rehabilitation. The existing historic structures, which include the underground vaults and a perimeter wall, will be modified in order to accommodate the new construction.

Site Restrictions

The proposed development includes the construction of 55,300 square feet of retail and cinema space along Bay Street at the Bay Street Level, and a 164-room hotel, banquet hall and a 96-unit residential building to be constructed at the Pier Level. As part of the proposed development, the existing historic buildings would be restored and rehabilitated for use as part of the proposed hotel, banqueting and retail program. As described below, all restoration work would comply with the Secretary of the Interior’s Standards for Rehabilitation. The existing historic structures, which include underground vaults and a perimeter wall, will be restored in order to accommodate the new construction.

The proposed Lighthouse Point development is constrained by numerous physical and jurisdictional requirements, including the topography, access, easements, conditions of historic structures, site geometry, zoning, and FEMA guidelines as follows:

Due to these project constraints, the proposed new construction must be located adjacent to and in some cases above the historic structures. The New York State Historic Preservation Office has determined that the scale and siting of the new construction creates an adverse impact on the historic buildings and structures. As part of the environmental analysis being prepared for the project, this analysis has been prepared to document the alternatives that would minimize the potential for an adverse impact on historic structures. Section II, below, summarizes that alternatives analysis and demonstrates that it is neither prudent nor feasible to undertake such alternatives.

Lighthouse Point Alternatives Analysis for Historic Resources

Topography

The site drops precipitously from a high point on Bay Street to the waterfront esplanade. The high point of the site is located at the southernmost point along Bay Street which is elevation 52'-0". Bay Street continues on a curve to the intersection with Borough Place at an elevation of 35'-0" and then further drops to the waterfront esplanade at elevation 12'-0". This street expands to the waterfront plaza and the pier.

Access Restrictions

The Department of Transportation limits vehicular access to the site to the southernmost point along Bay Street at the Bay Street Level. This location at the high point of the site requires a ramping street to reach the heart of the development which is contained within the Pier Level. All customer and service traffic is required to enter and exit from this location.

Easements

The Staten Island Railroad (S.I.R.R.) runs beneath the site and prohibits the development of building, including foundations, within the foundations within the easement. At 60 feet in width, the presence of the easement effectively eliminates all building above this right-of-way and separates the site into two development parcels: Pier Level and Bay Street Level.

Condition of Historic Structures

The goal of the project is to retain, restore and reuse all of the existing historic buildings and as much of the existing historic structures as is feasible. All of the historic buildings would be incorporated into the hotel, banqueting and retail uses of the project.

The existing buildings are in generally poor condition. The project site has been vacant and unused for decades, and the buildings have been minimally maintained during that period. The buildings' interiors are in very poor condition, with partial collapses of floors and stairs in some locations. For the most part, the interiors were historically utilitarian in nature, and no significant interior finishes survive intact. The facades of the Administration Building are in generally poor condition. The other buildings and the perimeter wall are in generally fair condition, but require substantial restoration in order to be put back in use. The existing buildings require new windows, doors and roofs (including new slate Mansard roof for the Administration Building).

All of the historic buildings would be restored and retained as individual structures. Existing building envelopes would be retained with minimal creation of new openings at the rear facades of the Administration Building, Barracks and North Warehouse in order to allow for connection to new buildings. These connections would be through discrete openings and would require minimal removal of masonry, typically dropping sills at window openings. On the interior, the buildings will be rehabilitated with new finishes throughout, and will retain most of their existing floor area. Small cutouts to create double-height spaces will be made in select locations.

Site Geometry

The site is an irregularly-shaped development lot containing 146,231 square feet (3.4 acres) that is generally L-shaped and incorporates lot lines along two mapped streets: Bay Street and Borough Place. The Bay Street lot line is an arc that leads into the Borough Place lot line. From that point, the development lot is defined by a tax lot subdivision that incorporates portions of the public esplanade and

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navigates existing conditions and existing buildings to connect with common lot lines with the neighboring Post Office.

The siting of proposed development on the zoning lot is constrained by both the historic structures and other unique site conditions. Three of the existing historic buildings (the Administration Building, the Barracks and the North Warehouse) front along the Waterfront roadway, leaving only a small area to the west as developable. The Laboratory Building and the Vaults are located within this area to the west of the North Warehouse. In addition to the sloping terrain, the developable area of the site is effectively subdivided by the SIRR easement running roughly north to south. The proximity to a working railroad, combined with the limited square footage available for the location of new buildings is a substantial limitation on development, both practically and in terms of integration with the historic fabric.

The Bay Street wall has a single opening, and thus blocks all potential retail development along Bay Street, a main thoroughfare in St. George. The Bay Street wall is constructed of brick piers with sections of wall three-brick widths thick between.

Zoning

The site is currently zoned C4-2 and is governed by waterfront zoning guidelines effecting height and location of structures. As indicated in the attached Site Analysis: Development Constraints document, the zoning requires varying building setbacks along each property line as to side and rear yards. In addition, the International Building Code with New York City Amendments requires further setbacks so that buildings have windows. These requirements limit the location of all development on the site.

Moreover, the base height of the proposed development is limited to 60 feet by the Zoning Resolution. Above this height the development is considered a tower and is limited in height to 175 feet, including any and all penthouse mechanical structures. Towers must step back at the top two floors and residential towers are limited to 8,100 gross square foot floor plates. Furthermore, the portion of the development rising above 60 feet in height is limited by zoning guidelines to no more than 100-foot wide as measured along the orientation of St. Marks Street two blocks to the west. Intended to preserve views of New York Harbor from higher locations within the St. George neighborhood, this 100-foot “tower corridor” creates a non-orthogonal development corridor through the site.

FEMA Guidelines

New FEMA Advisory Base Flood Elevations (ABFEs) for the entirety of New York City were published on February 24, 2013. A small portion of the site is located within the 500-year flood designation. As such, all building critical systems need to be located above this elevation.

Due to these project constraints, the proposed new construction would be located adjacent to and in some cases above the historic structures. NYSOPRHP has determined that the scale and siting of the new construction creates an adverse impact on the historic buildings and structures. As part of the environmental analysis being prepared for the project, this alternatives analysis has been prepared to document the alternatives which would minimize the potential for an adverse impact on historic structures. The following section summarizes that alternatives analysis and demonstrates that it is neither prudent nor feasible to undertake such alternatives.

II. Alternatives

Lighthouse Point Alternatives Analysis for Historic Resources

This alternatives analysis considers three alternatives to the proposed action, to examine reasonable and practicable options that avoid or reduce action-related adverse impacts and that may still allow for the achievement of the stated goals and objectives of the proposed action. Alternative 1 focuses the development density on the Bay Street Level site, providing the most separation from the historic structures along the esplanade. Alternative 2 assesses the viability of low-rise development along the esplanade at Pier Level behind the historic structures located there. Alternative 3 considers the options for the Bay Street Wall, which are applicable to Alternatives 1 and 2

Alternative 1: Upland Tower

The first design alternative that was considered was locating all tower elements on the Bay Street Level site. While this approach minimizes impacts to historic structures, it severely limits the developable area and would result in a reduction in the number of towers. This alternative would feature a two-level retail base, four levels of structured parking and a single residential tower above. Due to zoning height and other restrictions this development scenario would not allow for both the proposed hotel and residential programs. This scheme would result in 88,300 square feet of retail, and a single 77,500 square-foot residential tower that rises nine stories above the retail podium and contains 66 dwelling units. The location coupled with zoning height restrictions limits the number of residential units, and precludes the hotel program, thereby creating an economically unfeasible project.

This development alternative would also require the residential tower to be built within the first phase of the development, defeating the purpose of a phased development contemplated to allow for adjustment to the marketplace. Specifically the residential portion of the project has been placed in the third phase to allow for the retail on the Bay Street Level in Phase 1 to generate interest and activity in the site and its amenities therefore creating increased activity and value for the later residential component.

Furthermore, this alternative would also leave the historic buildings isolated from the rest of the project and not viable for reuse as restaurants, retail or other amenities that would be appropriate for the long-term viability of the historic buildings.

The existing historic structures are in an ever worsening condition with undetermined structural integrity. Existing floor to floor heights limit marketable land use options. The floors are too low to accommodate marketable and flexible retail space and therefore would require rent concessions for a limited tenant market that could utilize these conditions.

This Alternative represents a very limited land use program that is countercyclical to the marketplace and leaves the long-term viability of the historic structures in question and that significantly limits the economic viability. Taken together, this option eliminates the hotel entirely and reduces the residential component by about a third, creating an economically unfeasible project.

Alternative 2: Low-rise Waterfront Development

A second design alternative that was considered included retail on the Bay Street Level site and a low-rise residential and hotel development alongside and integrated into the historic structures. This development scheme resulted in large continuous building massing along the entire length of the site on its north-south direction. In addition to the detriments noted below, this scheme would require substantial demolition of the rear walls of the historic buildings and would integrate the new and old structures in a manner that would likely constitute a significant and irreversible adverse impact. A second design alternative that was considered included retail on the Bay Street Level site and a low-rise retail and hotel development that integrated the historic structures. This development scheme resulted in a large continuous building

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massing, stretching north to south, along the entire length of the site. In addition to the detriments noted below, this scheme would require substantial demolition of the exterior walls of the historic buildings.

This design alternative is inefficient from a building organization standpoint and is contrary to proper building orientation from an environmental and energy efficiency perspective. This alternative would result in 88,300 square feet of retail, a 33,800 square-foot, five-story residential building containing 22 dwelling units, and a 164,000 square-foot hotel with 68 keys and a limited banquet space. This alternative reduces both the hotel and the residential portion of the project by more than two-thirds each, and thus did not result in the required development program. Further, this alternative did not allow for efficient building layouts and sizes. Given the limited height of this scheme, the waterfront nature of the site was obliterated. Views to the waterfront and the New York City skyline beyond were extremely limited and at least half of the residential and hotel units would have no water views at all. This scheme would not result in the required development program nor would it allow for efficient building layouts and sizes. Moreover, this scheme would result in a large oppressive mass that eliminates any visual and physical connections for pedestrians to the Bay Street Level of development and Bay Street.

This low-rise development alternative fails to capitalize on the waterfront aspects of the site thus limiting the projects overall appeal and value to tenants and the public. Its organization would minimize the height of the development and the development yield of the site thus making for challenging economics.

Alternative 3: Bay Street Wall

This alternative would apply to the programmatic options of Alternatives 1 and 2 and would preserve the Bay Street wall in its entirety. In place of removing two sections of the Bay Street wall, this alternative retains the full extent of the existing wall, which would relocate the vehicular entrance to the site and prevent pedestrian entrance to all retail uses except at the southwest corner of the project site (approximately 150' from the reasonably anticipated entrance to the retail uses). Maintaining the full extent of the existing wall would conflict with the existing requirements of the St. George Special Zoning District, which require that commercial developments located within the Waterfront Subdistrict include an active streetfront with a minimum of 50% clear glazing along the street measured 10'-0" above the sidewalk (see attached Street Wall Transparency diagram). Further, maintaining the existing Bay Street wall with only one vehicular entrance and no pedestrian entrances would be economically unviable from a leasing standpoint in that it would result in rents that would not support the construction costs. Moreover, the design would create a suburban mall-like condition, wherein patrons would be encouraged to access the retail by automobile, and pedestrians going to and from the ferry would be discouraged from using the retail. The historic wall along the curved frontage at Bay Street represents a historic amenity and yet a barrier to a pedestrian-friendly and engaging streetscape. We have proposed judiciously and selectively eliminating discreet panels, approximately 17'-0" each, on the wall to provide street access to the retail storefronts. Respecting the structural integrity of the wall, the current design proposal eliminates the northeastern and southwestern ends to provide site and retail access. Additionally, the proposed openings within the length of the wall would be located between existing brick piers thus maintaining the structural cadence along Bay Street.

Retention of the full extent of the wall along Bay Street would relocate vehicular entrance to the site and prevent pedestrian entrance to all retail uses except at the southwest corner of the project site, which would be approximately 150 feet from the reasonably anticipated entrance to the retail uses. Maintaining the wall's exterior, with only one entrance, would conflict with zoning regulations, which stipulate under the St. George Special Zoning District that commercial developments located within the Waterfront Subdistrict include an active street front, with a minimum of 50% clear glazing along the street. Furthermore,

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maintaining the Bay Street wall with only one entrance would be economically unviable from a leasing standpoint in that it would result in rents that would not support the construction costs. Moreover, this design would also create a suburban mall-like condition, wherein patrons would be encouraged to access the by automobile, and pedestrians going to and from the ferry would be discouraged from using the retail. Our proposal attempts to be sensitive to both issues of historic integrity and enhanced pedestrian-friendly streets.

It is difficult to determine the discount in potential rent for these retail units, given that the marketing of these units would be challenged given no access from Bay Street, the main retail street. We would be limited to a tenant that would be a true destination retailer and be able to absorb the majority of the space. The potential loss could be in excess of \$15 psf. or \$750,000 annually.

Understanding the inherent conflicts between DOT access requirements, zoning guidelines, historic concerns, and leasing strategies, maintaining the wall in its entirety would alter the vehicular entrance location, develop a non conforming use with respect to zoning guidelines and create unmarketable or at least less desirable retail tenants spaces. Due to these concerns, it was concluded that this option was not feasible.

III. Conclusion

As proposed, the Lighthouse Point development will bring new and vibrant uses to an underutilized site. The scale and density of the proposed development are appropriate to its location, and necessary to create an economically viable project that includes substantial rehabilitation of the historic structures. The massing of the new buildings takes advantage of the site's many amenities while at the same time working within the confines of the many site constraints. The site's constraints, which include the location of the historic structures in relation to the site topography and infrastructure, require that the new construction sit adjacent to and in some case over the historic structures, and further require selective openings in the Bay Street wall. However, by weaving new and old in a creative manner, all of the existing historic buildings are allowed to remain as freestanding, largely independent structures. Interventions such as new openings in exterior walls and openings within the Bay Street wall are selective and readable. None of the alternatives studied are economically viable, and result in a design that is less desirable and ultimately more harmful, physically and economically, to the historic structures.